

# CHELSEA

## Outline Plan and Land Use Redesignation

March 2025

Submitted by: **UNITED COMMUNITIES**

Prepared by: **B&A PLANNING GROUP**





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**Westhoff  
Engineering  
Resources, Inc.**

*Land & Water Resources Management Consultants*

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## Section 1.0

# Introduction



## 1.1 Welcome to Chelsea

This report has been prepared on behalf of United Communities in support of their Chelsea land use and outline plan application. The proposed community of Chelsea encompasses all of Section 9-24-28 W4M, approximately 640 acres of land, south of Chestermere Boulevard. United Communities (United) owns the NW 1/4 and SW 1/4 of Section 9 which are the subject of this application. When reference to “Chelsea” or “United’s Chelsea” is made within this document, the reference is to United’s landholding only, unless otherwise noted. The remainder of Chelsea, the NE 1/4 and SE 1/4 Sections, will be subject to a separate outline plan and land use redesignation by others. The two outline plan areas have been collaboratively prepared.

United’s Chelsea comprises the W 9-24-28 W4M, is located at the southeast corner of the intersection of Chestermere Boulevard (Highway 1A) and Range Road 284 and is approximately 127.9 ha (316.2 ac) in size as identified on the land titles. The vision for Chelsea draws upon the appeal of the City of Chestermere: year round, family-focused, resort-inspired living where the natural and built-form environment complement each other. This is to be achieved by accommodating a number of community elements including a variety of dwelling types, a commercial centre, a neighbourhood node, schools, playfields and parks and a protected wetland and drainage course. The benefits of the Chelsea community will be supported by connections between various open spaces, a pedestrian and transit friendly environment and efficient transportation and servicing. To implement the vision, the subject site is to be redesignated to districts that allow for residential, commercial, mixed use and open space land uses. This land use and outline plan application is in alignment with the policies of the City of Chestermere including the Municipal Development Plan, Waterbridge Master Area Structure Plan (MASP) and the Gateway Area Structure Plan (ASP). The following provides specific details on the aspects of this new community.



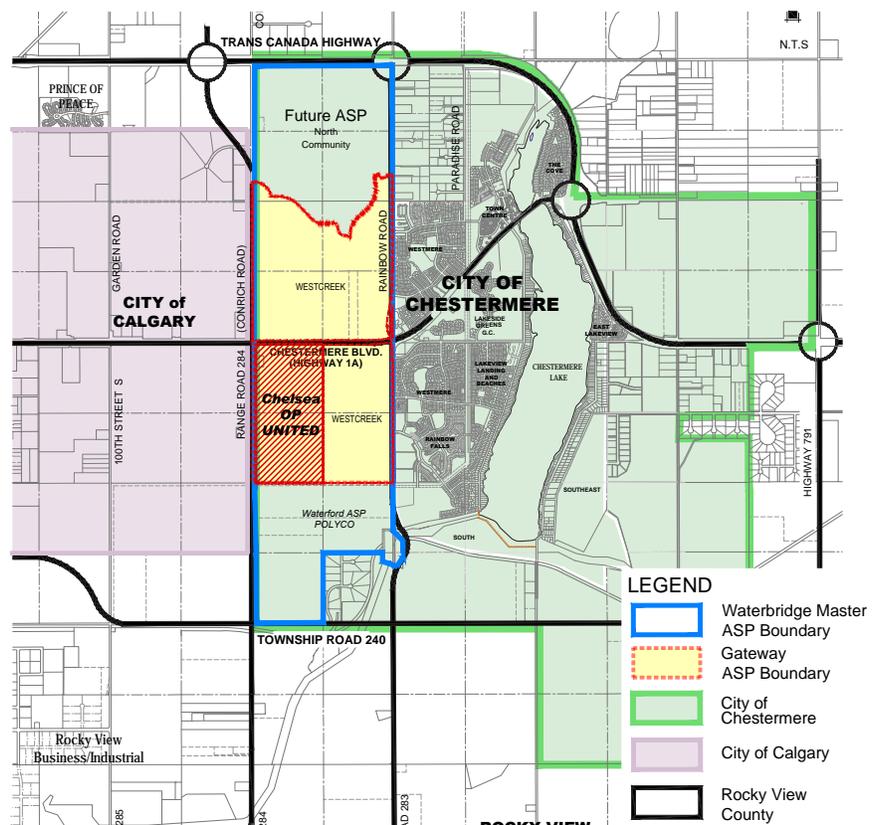
# Section 2.0 Site Context

## 2.1 Location

The subject site is located within the west portion of the City of Chestermere and directly adjacent to the border between the City of Calgary and the City of Chestermere. The intersection of Chestermere Boulevard (Highway 1A) and Range Road 284 forms the northwest corner of the subject site. The subject site extends south of Chestermere Boulevard and parallel to Range Road 284 to an Altalink power line which delineates the southern boundary of Section 9-24-28 W4M.

The subject site falls within the Waterbridge Master Area Structure Plan (MASP) and the Gateway Area Structure Plan (ASP). The site encompasses the west half of the South Central Community 'C' as identified in the Waterbridge Master Area Structure Plan.

Figure 1  
Location





## 2.2 Legal Description

Three parcels of land totalling 127.9 ha (316.2 ac) are included within the subject site:

Legal Description	Hectares	Acres
Block 1, Lot 1, Plan O210141	1.6	4.0
NW 9-24-28 W4M Excepting Plan 1766 LK and Plan O210141	61.6	152.2
SW 9-24-28 W4M	64.7	160.0
<b>TOTAL</b>	<b>127.9*</b>	<b>316.2*</b>

Note: \*The exact area will be determined at the time of subdivision. This area slightly differs from that shown on the outline plan.

## 2.3 Existing Conditions & Site Constraints

The subject site is mainly comprised of agricultural land. Other important elements include:

- A farmstead located in the extreme northwest portion of the subject site.
- Wetlands are distributed throughout the site. As per the Gateway Area Structure Plan (Section 10.5), only the major wetland in the southeast portion of the subject site will be preserved.
- An ephemeral drainage course cuts across the southwestern portion of the subject site.
- Situated in the southwest corner, along the western and southern property boundary, is a tree stand.
- A decommissioned abandoned well is located in the central - eastern portion of the site.

The land is relatively flat with slopes no greater than 5%.

Figure 2  
Existing Conditions



Legend

----- Chelsea OP boundary

Figure 3  
Slope Analysis



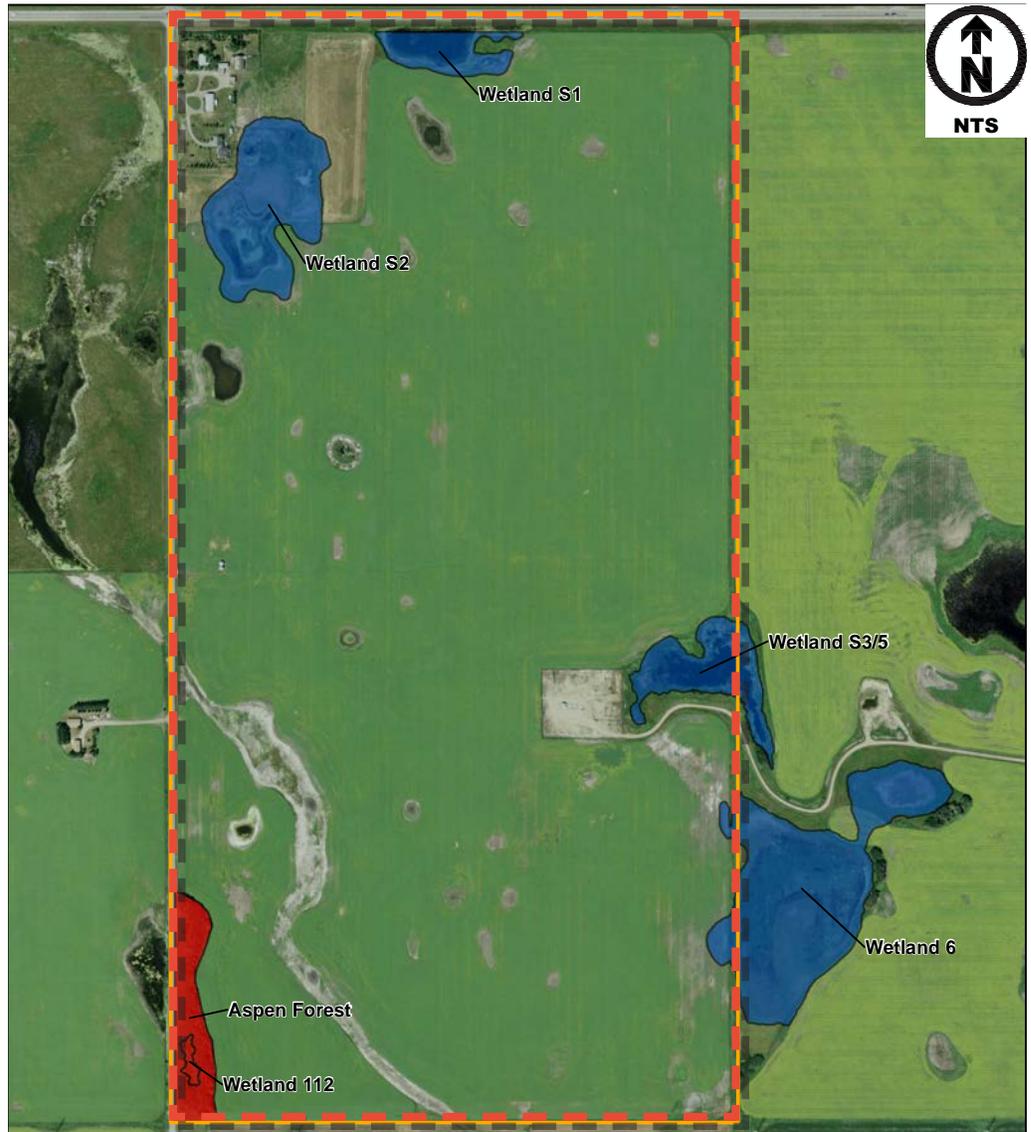
Legend

----- Chelsea OP boundary



2.5 - 5% Slope

Figure 4  
**Environmentally Significant Areas**



**LEGEND**

- Project Site
- ESA**
- Local
- Provincial

Imagery Source: Valtus

Client: UNITED COMMUNITIES			
Project: CHELSEA BIOPHYSICAL IMPACT ASSESSMENT			
Title: ENVIRONMENTALLY SIGNIFICANT AREAS			
Date: 13-04-2016	Project No.: WER114-30	Scale: 1:8,000	<b>App A</b>
<b>Westhoff Engineering Resources, Inc.</b> Land & Water Resources Management Consultants			

## 2.4 External Transportation Network

Bordering the subject site is Chestermere Boulevard (Highway 1A) to the north and Range Road 284 to the west. These two roads provide access to both the local and regional transportation networks. The following provides additional information on each road:

- Chestermere Boulevard (Highway 1A): This road runs east-west along the north boundary of the subject site and provides a connection between Calgary and the centre of Chestermere.
- Range Road 284: This road runs north-south along the west boundary of the subject site and provides a connection between the Trans Canada Highway to the north and Highway 560 to the south.

## 2.5 Adjacent Land Uses

The land surrounding the subject site is mainly undeveloped and used for agricultural purposes. North of the subject site and on the other side of Chestermere Boulevard are agricultural land, a farmstead and a wetland. To the east are agricultural land and a wetland. To the south and across the AltaLink power line are agricultural land, a farmstead and an ephemeral drainage course. To the west and on the other side of Range Road 284 are agricultural land, two farmsteads, an ephemeral drainage course and wetlands.



## Section 3.0

# Policy and Land Use Context



The following provides a summary of the applicable municipal policies that govern the development of the subject site. A demonstration of development's policy conformance is found in Appendix A: Policy Conformance.

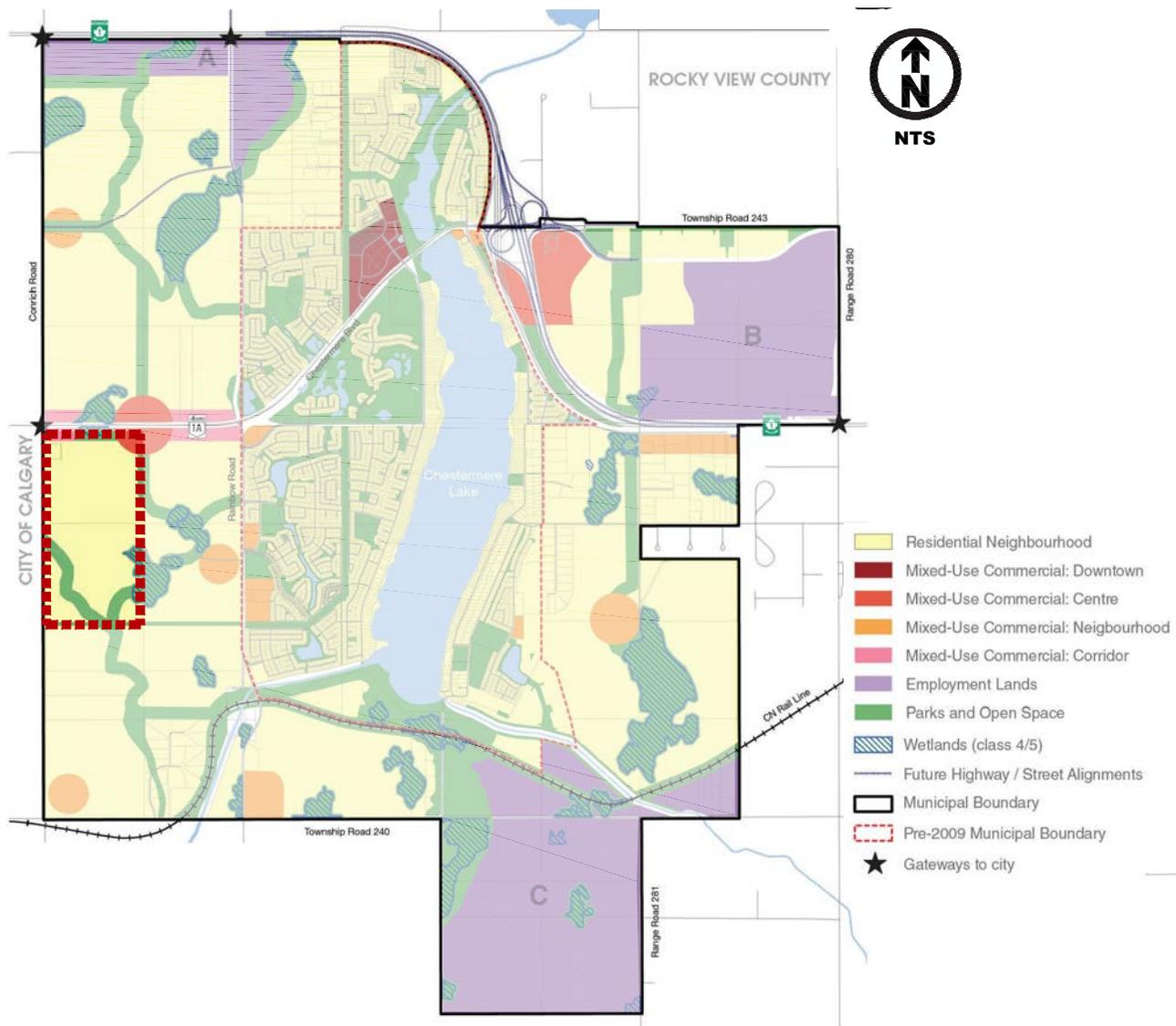
## 3.1 Municipal Development Plan

The City of Chestermere Municipal Development Plan identifies the land use and development goals, objectives, and policies that are intended to guide the future actions and decisions of City Council and the administration of the City of Chestermere. The MDP sets forth a policy and regulatory framework for the development of public and private land within the City. The MDP identifies important infrastructure and servicing issues for the City and sets out policies to achieve appropriate development within the community. The MDP identifies the following policy areas for the subject site:

- Residential Neighbourhood;
- Mixed-Use Commercial Centre;
- Parks and Open Space;
- Wetlands (Class 4/5); and
- Gateway to City.



Figure 5  
Municipal Development Plan

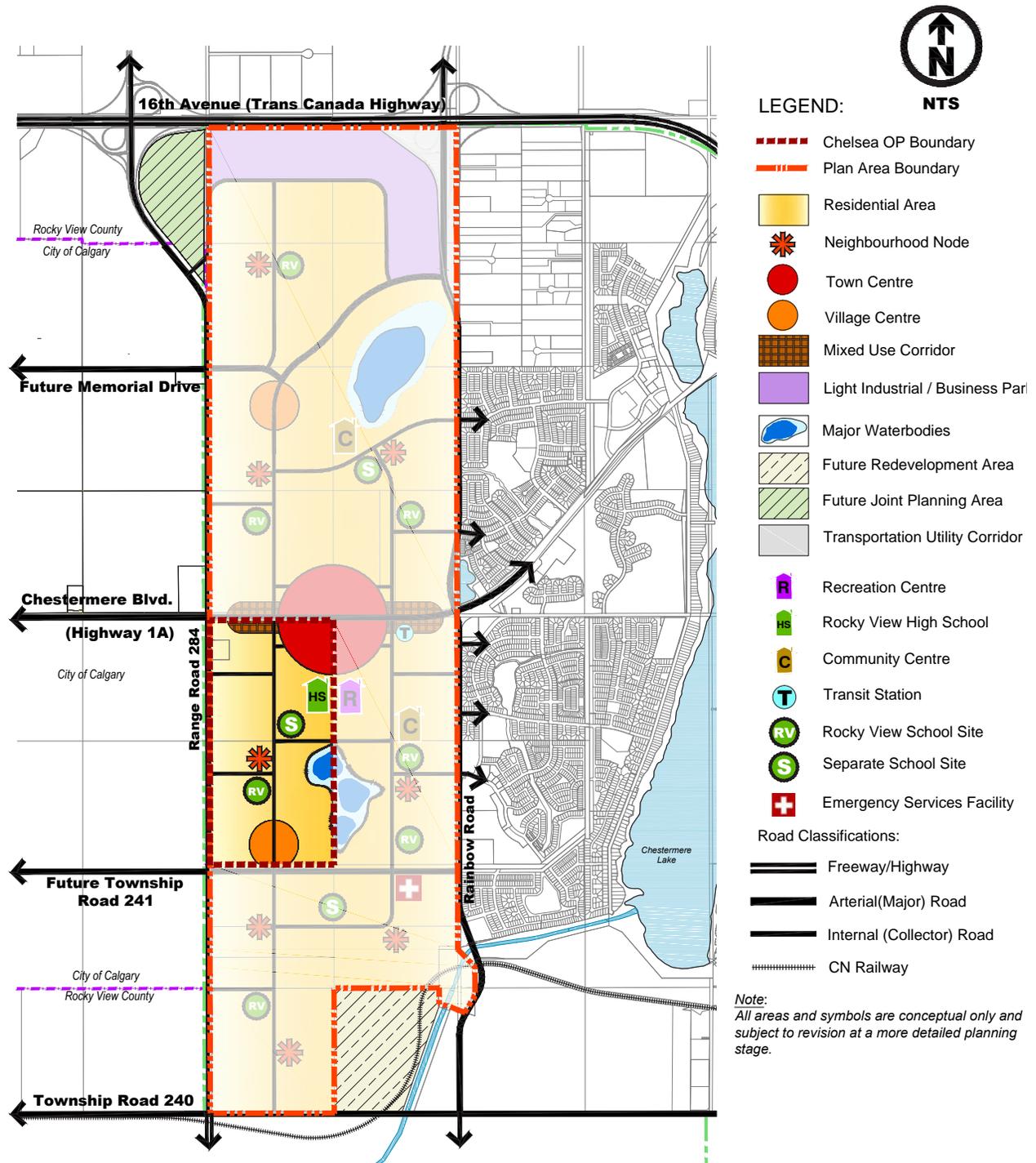


## 3.2 Waterbridge Master Area Structure Plan

The subject site falls within the Waterbridge Master Area Structure Plan (MASP). The intention of the MASP is to refine and implement the broader policies of the City of Chestermere's MDP and to guide and direct the preparation of subsequent Area Structure Plans (ASPs) and the approval of outline plans, land use amendments, subdivision and development permits applications. The MASP governs an area that is 971 ha (2,399 ac) in size that is bounded between Highway 1 to the north, Rainbow Road to the east, Township Road 240 to the south and Range Road 284 to the west. The MASP divides the subject site into a variety of policy areas:

- Residential Area;
- Neighbourhood Node;
- Village Centres;
- Mixed Use Corridor;
- Town Centre;
- School Sites; and
- Major Waterbodies.

Figure 6  
Waterbridge Master Area Structure Plan



### 3.3 Chestermere Gateway Area Structure Plan

The Chestermere Gateway Area Structure Plan (ASP) refines the broader policies contained in the City of Chestermere Municipal Development Plan (MDP) and the Waterbridge Master Area Structure Plan (MASP) and provides a framework for future community development. The ASP also guides and directs the preparation of subsequent Outline Plan and Land Use Amendment applications within the Plan Area. The ASP divides the area into Land Use Concept Components and provides policy direction for each. The Concept Components expands upon the policy areas of the MASP. The site falls into the following components:

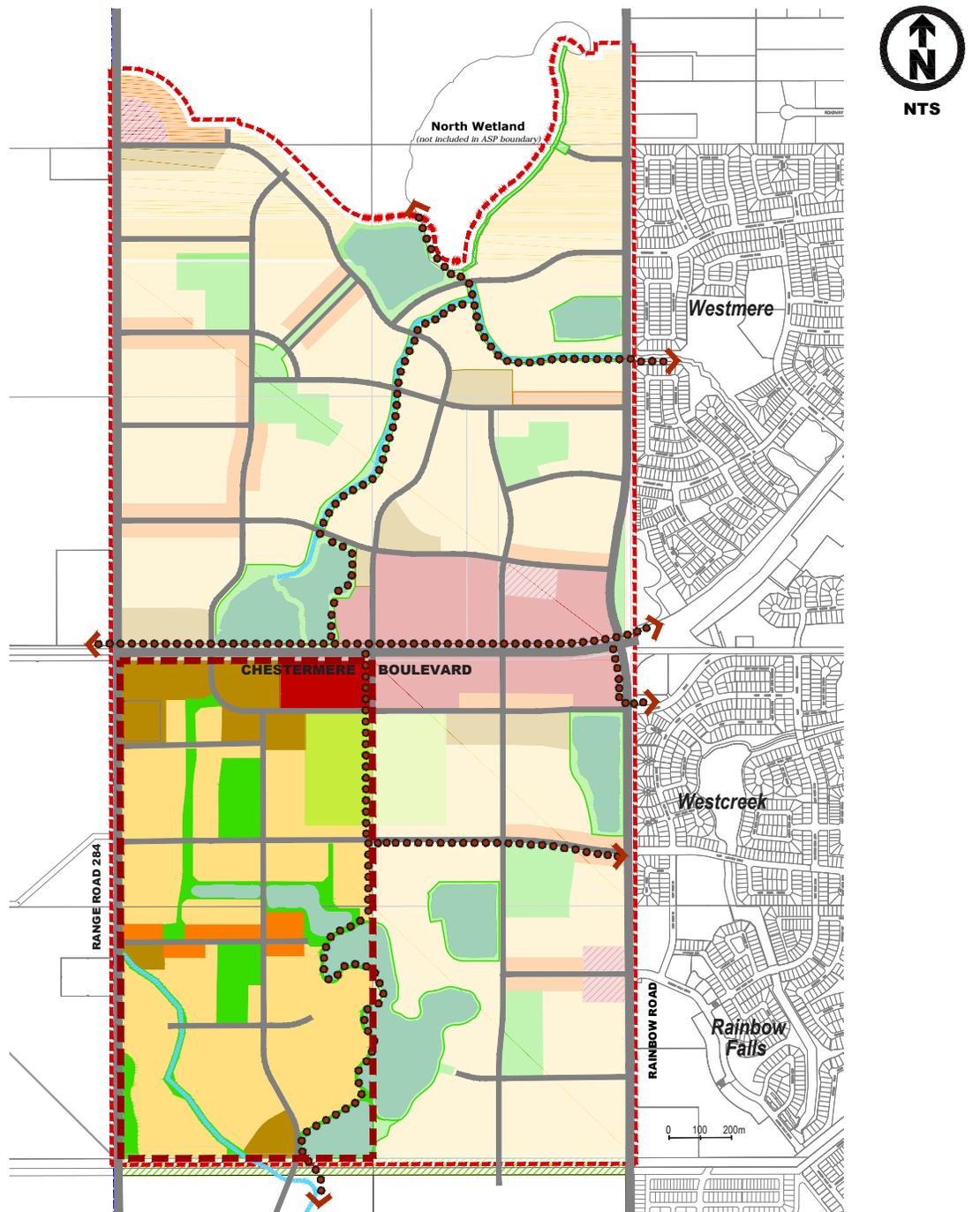
- Residential Area;
- Neighbourhood Nodes;
- General Commercial Area;
- Transitional Use Corridor;
- High School Site;
- School Sites;
- Major Wetlands;
- Regional Pathway; and
- Stormwater.

### 3.4 Current Land Use Designation

The subject site includes two land use designations:

- The area corresponding to the existing farmstead is currently designated Large Lot Rural Residential (LLR) district. The purpose of this District is to allow for residential and a limited range of agricultural and rural land use activities that do not prejudice the future use of the lands for urban use.
- The majority of the subject site is currently designated Urban Transition (UT) under the Land Use Bylaw. The purpose of this District is to preserve land in an environmentally sustainable manner and in a relatively undeveloped state awaiting urban development and utility servicing, while allowing a limited range of temporary uses and recognizing existing agricultural operations.

Figure 7  
Chestermere Gateway Area Structure Plan



## LEGEND

 CHELSEA OP BOUNDARY	 REGIONAL PATHWAY	 HIGH DENSITY RESIDENTIAL
 GATEWAY AREA STRUCTURE PLAN BOUNDARY	 NATURALIZED DRAINAGE COURSE	 MIXED USE (COMMERCIAL/RESIDENTIAL)
 CITY OF CALGARY / CITY OF CHESTERMERE BOUNDARY	 O/H TRANSMISSION LINES	 TOWN CENTRE
 MAJOR ROADS / COLLECTOR ROAD	 LOW DENSITY RESIDENTIAL	 MAJOR OPEN SPACE
 COUPLER ROAD	 MEDIUM DENSITY RESIDENTIAL	 RECREATIONAL CENTRE / HIGH SCHOOL
		 STORM WATER TREATMENT SYSTEM AND WETLANDS

## 3.5 Chestermere Boulevard Corridor Plan

The Chestermere Boulevard Corridor Plan sets out a vision for Chestermere Boulevard and provides guidance and concepts for how to achieve it. The concepts are to support and synergize with new development, invigorate existing commercial uses and connect residents to community amenities and each other. Under this plan, Chestermere Boulevard is envisioned to function as an urban street that is safe for all users, facilitates local connections and serves as a gateway to the municipality.

Chestermere Boulevard runs along the northern boundary of the subject site. This portion of Chestermere Boulevard is identified as the Western Gateway and the Town Centre segments (see Figure 9). The urban design and landscape architecture elements along the Western Gateway segment should provide distinguishing features and a streetscape that indicates a sense of arrival. The Town Centre segment is intended to carry high volumes of traffic while also providing frontage and access for businesses and other land uses. Once upgraded, the two segments are expected to accommodate vehicles, cyclists, pedestrians and transit.



Figure 8  
Western Gateway and Town Centre Cross Section

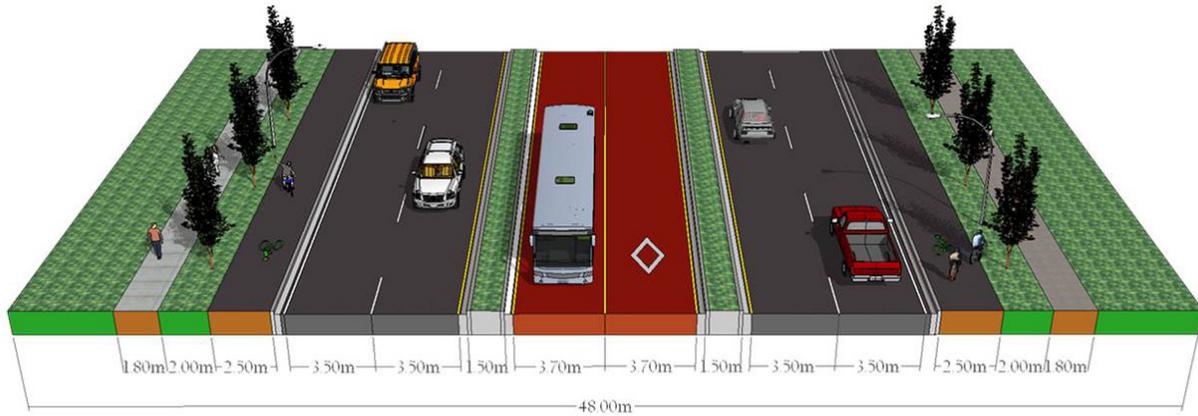
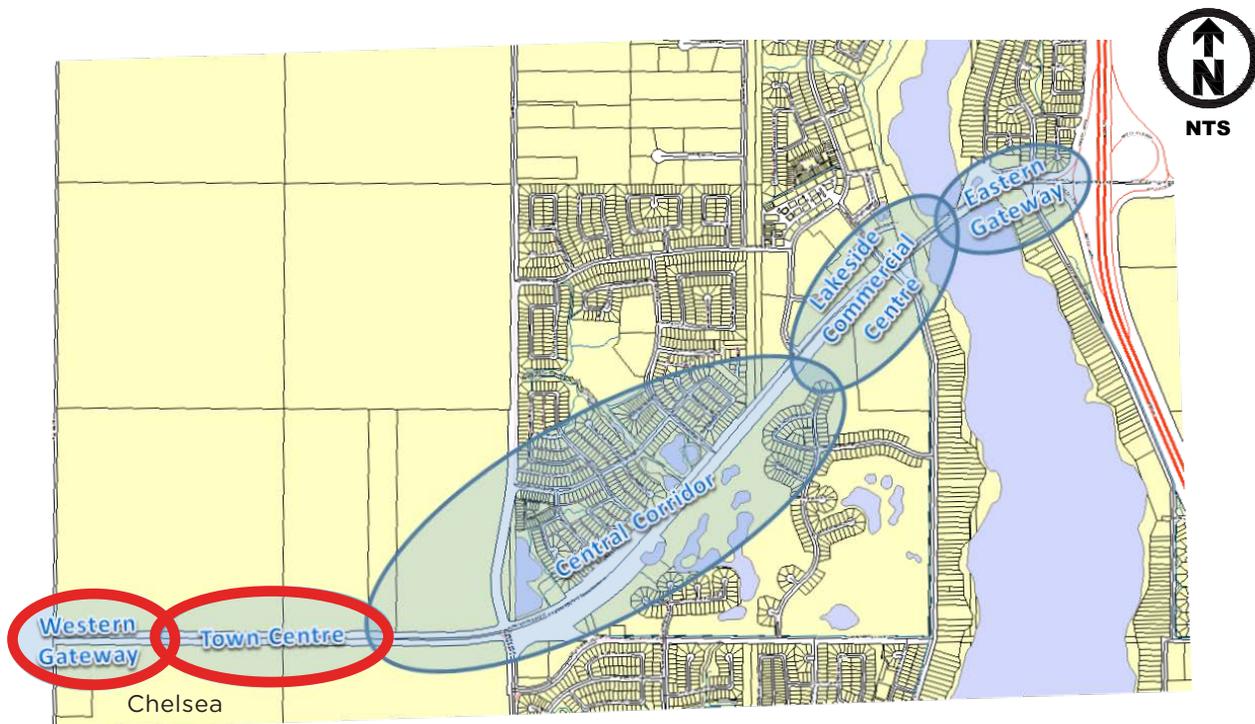


Figure 9  
Chestermere Boulevard Corridor Segments



## Section 4.0

# Community Name and Identity

## 4.1 Community Naming

The name Chelsea is derived from the Saxon words *cealc hythe*. The word *hythe* meant a landing place for boats. The word *cealc* meant chalk so “landing place [on the river] for chalk or limestone.”

If you were to deconstruct the meaning of Chelsea, the word is fitting for the location, character and vision of the site. *Landing* ties into the intention of this area being a gateway to Chestermere. *On the river* is a reference to the water elements to be incorporated into the development including the natural drainage course, the wetland and storm ponds.

Chelsea’s identity draws from the same qualities that set the City of Chestermere apart by placing an added value on family, recreation and the great outdoors.

Inspired by 1950’s and 60’s style family-friendly resort havens, coastal towns and nature’s biggest playgrounds, every facet of Chelsea’s development – from brand to built form – will endeavour to feel intangibly different and yet comfortable in its surroundings.

Bold welcome signage, landscaped streets and blossoming hanging baskets on storefronts will set the community apart and harken back to family getaways in destination resort towns.

Ponds and environmental reserve will blend into the landscape and provide for meandering paths and boardwalks that respect wildlife and honour the great outdoors.

A variety of housing types will line the streets with a coastal flair and special attention paid to the landscape and natural surroundings.

Chelsea is intangibly different. And soon to be familiar in Alberta’s Recreation and Relaxation Oasis.





## Section 5.0

# Vision and Principles

The vision for Chelsea draws upon the appeal of the City of Chestermere: year round, family-focused, resort-inspired living where the natural and built-form environment complement each other. This is achieved by accommodating opportunities for community recreation, shopping and education, homes that maintain a comfortable lifestyle and a close and personal connection with nature and water. The benefits of the Chelsea community are supported by links between various open spaces, a pedestrian and transit friendly environment and efficient transportation and servicing.



### Principle 1: **Chelsea is an Escape**

---

Chelsea promotes relaxation, an active lifestyle and pleasure through its diversity of residential, commercial and open space uses, its urban design and its architecture.



### Principle 2: **Integration of Nature and Water**

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Nature and water are key place-making elements that are incorporated into the community to support Chelsea`s waterscape theme.



### Principle 3: **Recreational Focused Open Space System**

---

Green corridors connect the various green infrastructure components of the community into a unified open space system.



Principle 4:  
**Pedestrian and Transit Friendly**

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A comprehensive network of sidewalks, pathways and trails allow for safe pedestrian circulation and access to amenities and transit.



Principle 5:  
**Efficient Road Network**

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The road network is a modified grid that moves vehicles efficiently through the community and provides effective connections with the regional road system.



Principle 6:  
**Appropriate Servicing**

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Servicing including water, sanitary and stormwater infrastructure meets the needs of the community.

## Section 6.0

# Chelsea Concept

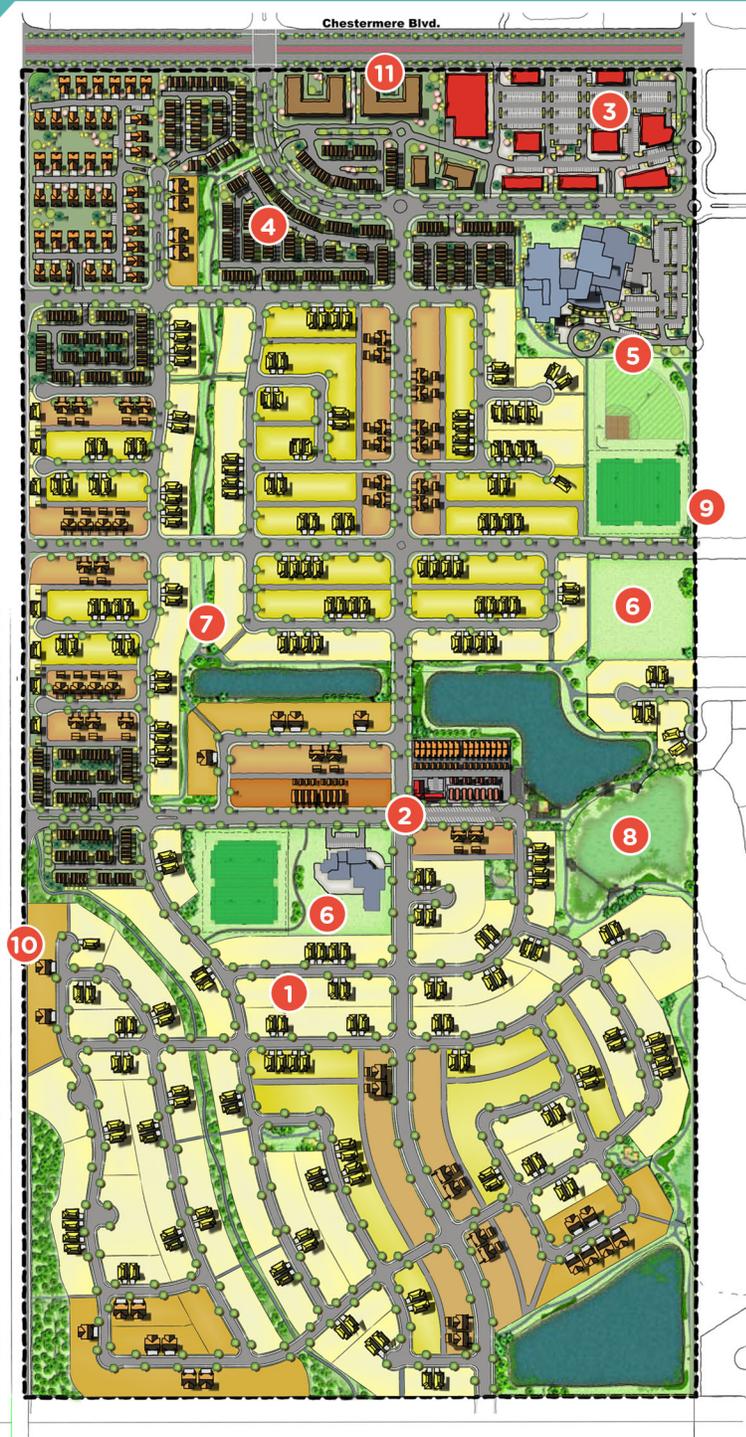


Figure 10  
**Overall Concept Plan**

Figure 10 and the following sections provide a conceptual overview of the main elements of the envisioned development. These elements include:

1. Residential;
2. Neighbourhood Node;
3. General Commercial Area;
4. Transitional Use Corridor;
5. High School Site;
6. School Sites;
7. Parks;
8. Eco Park;
9. Regional Pathway;
10. Range Road 284 Interface; and
11. Chestermere Boulevard Interface.

At the end of each section, relevant municipal policies are identified to demonstrate how the project complies.



## 6.1 Residential

The majority of the site will comprise of residential development. Chelsea residents will be able to choose from a number of different dwellings types including:

- Single detached;
- Semi-detached dwellings;
- Duplexes;
- Townhouses;
- Stacked townhouses;
- Apartment buildings; and
- Secondary suites.

Higher density residential will be placed strategically to support the Neighbourhood Node, General Commercial Area and the Transitional Use Corridor. This housing choice will allow families to find a home that meets their particular lifestyle at any stage in life and lead to a stable and diverse community.

### Relevant Policies:

*Municipal Development Plan: Residential Neighbourhood is intended to create complete communities with distinct characteristics, boundaries, and elements that form livable, vibrant neighbourhoods and identity.*

*Waterbridge Master Area Structure Plan: The purpose of the Residential Area is to accommodate predominantly low density residential development, together with a wide range of complementary and compatible uses that serve the social, recreational, spiritual and civic needs of residents. Different housing forms, such as live/work units, secondary suites and multi-dwelling projects will also be allowed, where appropriate.*

*Gateway Area Structure Plan: The Residential Area accommodates predominantly single-detached dwellings, but also allows for compatible multi-family residential development and other complementary neighbourhood-scale uses.*

## 6.2 Chelsea Village (Neighbourhood Node)

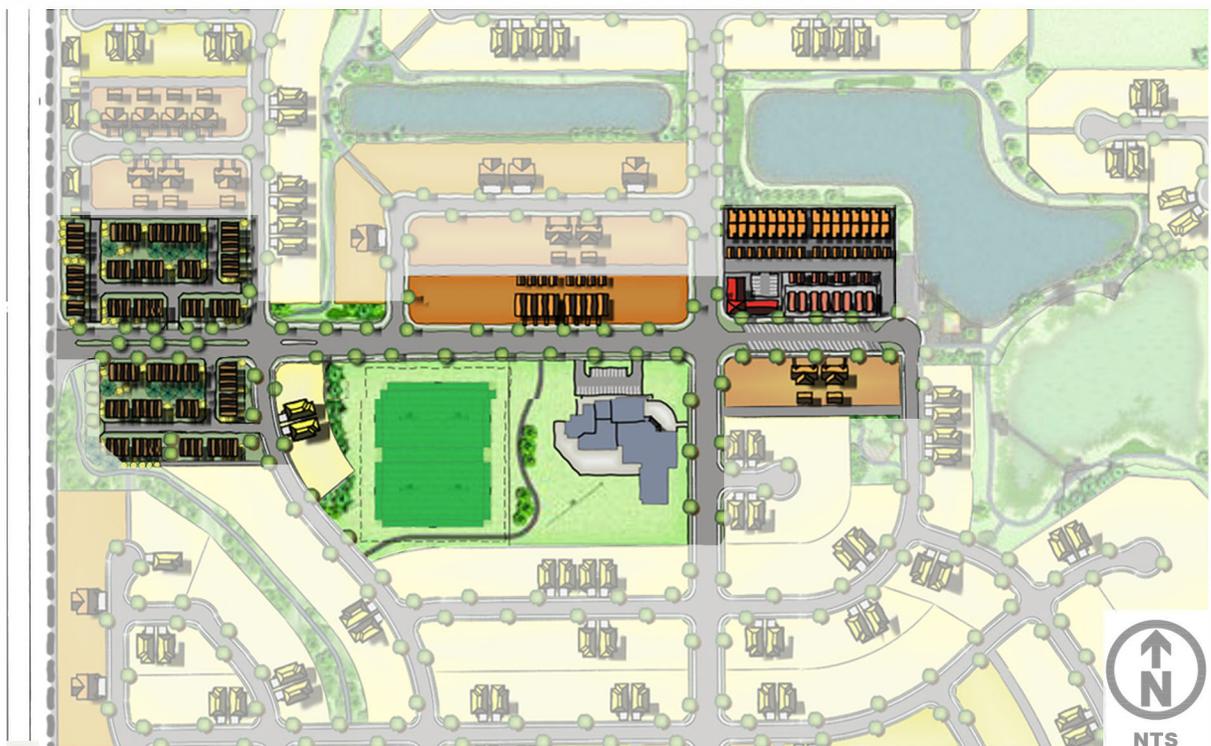
A Neighbourhood Node, Chelsea Village, will be established in the centre of the community at the intersection of Chelsea Drive and Chelsea Parade. Chelsea Village will be orientated along Chelsea Park Walk and between Range Road 284 and the Eco Park. The western portion of the node will include apartments, townhouses and semi-detached dwellings and a school site and a playfield. The eastern portion of the node will accommodate a comprehensively designed mixed use site, integrating residential and commercial components. It provides a gateway into neighbourhood node and terminate at a park that includes a plaza, a stormpond, the Eco Park and the Regional Pathway. The Neighbourhood Node will be the heart of the community and bring together people for shopping, recreation and education.

### Relevant Policies:

**Waterbridge Master Area Structure Plan:** *The purpose of a Neighbourhood Node is to provide a social and activity focus for the surrounding Neighbourhood Area. Neighbourhood Nodes will be comprised of a mix of residential and non-residential uses situated around a central park or plaza and capable of being served by future public transit.*

**Gateway Area Structure Plan :** *These nodes will serve as the focal point of the Neighbourhood, and contain central park space, multi-family residential housing and a future transit stop.*

Figure 11  
Chelsea Village Concept Plan





Local Commercial



Mixed-Use



Plaza



Townhouses

Note: Conceptual and subject to change



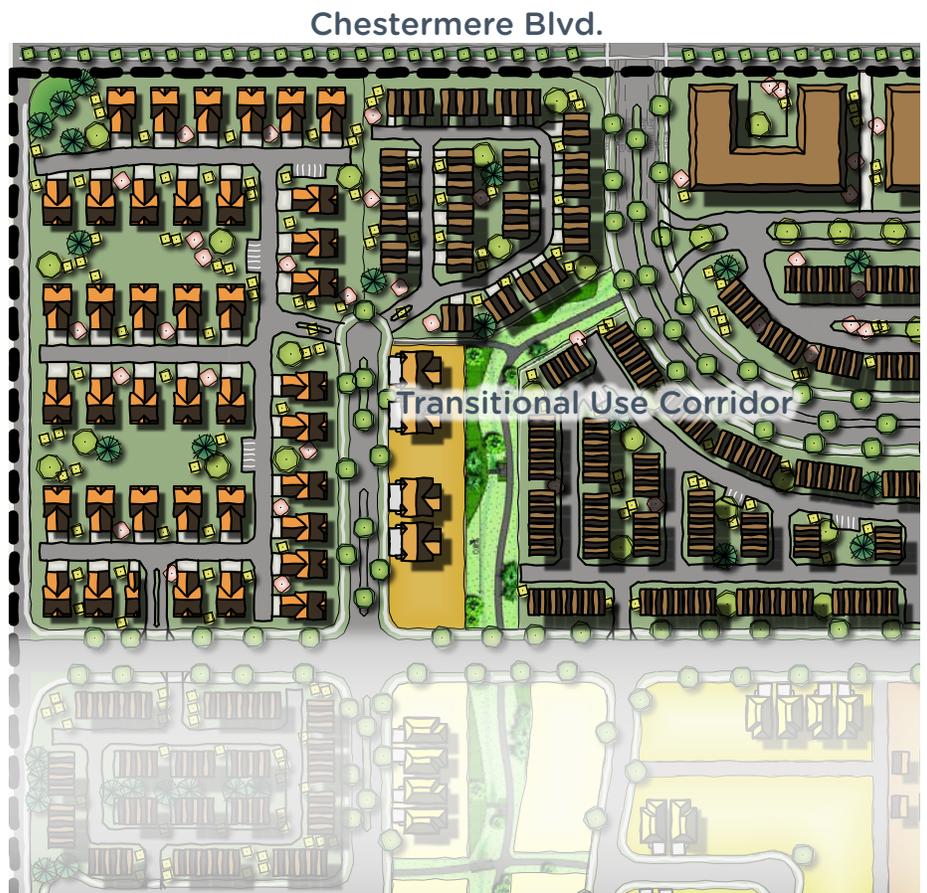
Note: Conceptual and subject to change

## 6.3 General Commercial Area

The Chelsea Commercial Centre is envisioned to be a regional and community focal point that will provide daily commercial and service needs in a walkable, human scaled commercial centre. Grocery, retail, service and restaurant sites are primarily located on the south and west sections of the commercial site, enabling walkability between these services and the residential areas. To ensure an attractive interface with Chestermere Boulevard, some commercial buildings will be located directly adjacent to Chestermere Boulevard to buffer the visual impact of the parking areas. All parking areas will conform to parking lot design requirements found in the Chestermere Design Guidelines. The businesses established within these commercial spaces will accommodate local services and daily needs leading to local employment and convenience. The General Commercial Area is intended to complement the other components of the Town Centre including the High Street Area, Transitional Use Corridor, Recreation Site and the High School.

Prior to development, a Development Permit will be required. Along with the Development Permit application, a detailed master site plan will be required which will show building siting, parking locations, pedestrian linkages, landscaping, architectural controls and guidelines and other site design details as required by the Approving Authority and endorsed by Council. See Appendix B for specific Commercial Design Guidelines.

Figure 12  
General Commercial Area  
and Transitional Use Corridor  
Concept Plan

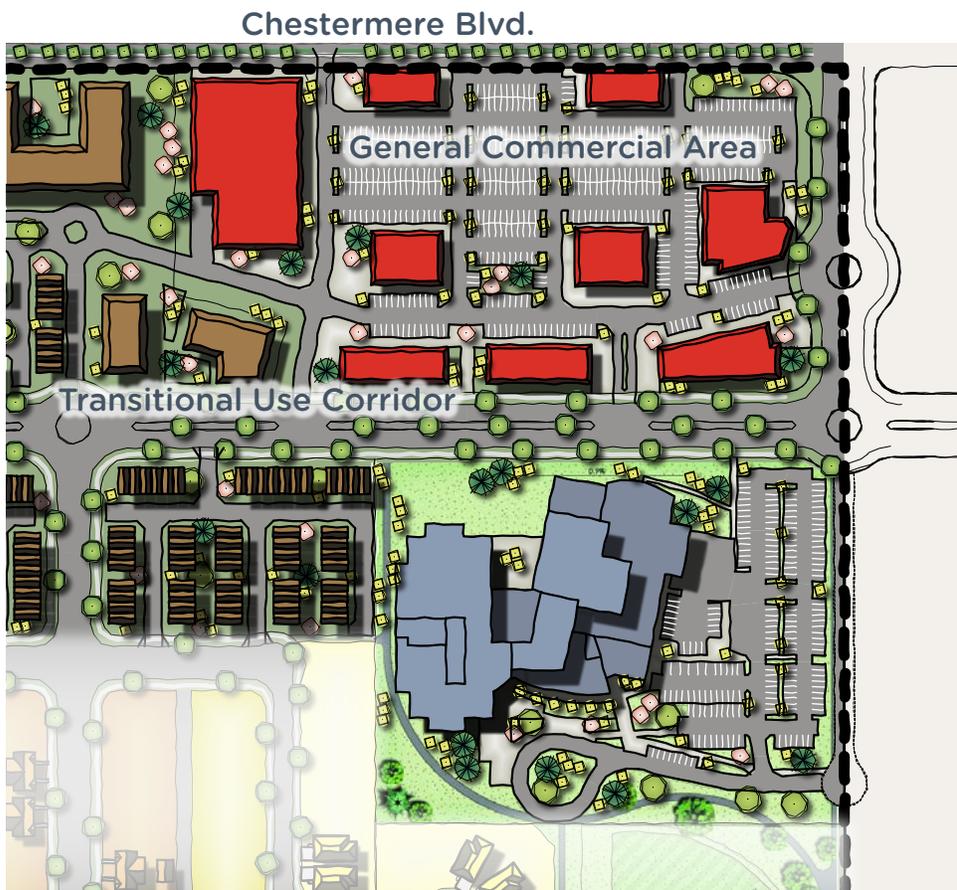


## Relevant Policies:

**Municipal Development Plan:** *The intent for the Mixed-Use Commercial: Centre is to complement the existing Mixed-Use Commercial: Downtown by providing regional and city-wide retail and service offerings. Planning for these areas shall recognize the potential for key nodal development along major transportation corridors that are well-posed in the regional market.*

**Waterbridge Master Area Structure Plan:** *The General Commercial Area will accommodate larger format retail stores, and will serve as a regional draw that benefits from high visibility and direct access along Chestermere Boulevard.*

**Gateway Area Structure Plan:** *The Town Centre will contain a General Commercial Area to include small, medium and larger format retail uses located within a series of blocks defined by intersecting public roads. These retail uses will include anchor tenants that attract shoppers to the area. Larger format commercial uses should present a street-oriented design that supports the function of the centre.*



## 6.4 Transitional Use Corridor

West and south of the General Commercial Area will be the Transitional Use Corridor. This corridor will be made up of five multi-residential parcels composed of a comprehensively designed semi-detached dwelling, townhouse and apartment developments. The corridor will provide a complementary transition between the General Commercial Area and the lower density residential. The units fronting Chestermere Boulevard and the future gateway entry feature will provide a distinctive welcome to the City. The higher residential densities are strategically located in order to bring a larger number of residents within walking distance of the General Commercial Area and the High School.

In the northwest corner of the Transitional Use Corridor, a parcel of land will be set aside for the municipality to accommodate a City of Chestermere entry feature. The design, construction and maintenance of the parcel and entry feature will be the responsibility of the City of Chestermere. The surrounding development will offer complimentary architecture and landscaping that provides an attractive backdrop for the entry feature. Along with the Development Permit application, a detailed master site plan will be required which will show building siting, parking locations, pedestrian linkages, landscaping, architectural controls and guidelines and other site design details as required by the Approving Authority and endorsed by Council.

### Relevant Policies:

*Municipal Development Plan: The intent for Mixed-Use Commercial: Corridor is to recognize the potential for more intensive residential, commercial and institutional uses along arterial corridors, and encourage the development of multi-modal corridors through core areas of the city.*

*Waterbridge Master Area Structure Plan: The purpose of the Mixed Use Corridor is to accommodate a mix of residential development along Chestermere Boulevard to the east and west of the Town Centre. In addition, commercial, institutional, recreational and similar uses that are complementary to the area, and do not compromise the viability of the Town Centre, are also considered to be appropriate. Commercial uses may include service commercial and secondary commercial uses. As well, horizontal and vertical mixing of uses, either within a building or on a site, are encouraged in the area.*

*Gateway Area Structure Plan : This area will accommodate predominantly high to medium density residential developments consisting of townhousing and walk-up apartments, together with the potential for local commercial uses, secondary suites, live-work units and other compatible uses. The mixed use corridor will complement and support the Town Centre.*

## 6.5 Open Space

The Chelsea open space network consists of a series of connected green spaces creating two green loops within the community. The loops will accommodate pedestrians and cyclists through the pathways, sidewalks and on street bike lanes embedded in the loops.

- **North Loop:** Commencing from the Transitional Use Corridor, a green corridor runs south to connect to two stormwater ponds that run east-west. The stormwater ponds subsequently link to a green corridor that runs north to connect the High School with the General Commercial Area. The loop is completed through a sidewalks located on Chelsea Boulevard.
- **South Loop:** Commencing from the stormwater pond located in the southeast portion of the site, a green corridor runs along the eastern boundary of the site to connect the Eco Park with the two east-west stormponds. The loop subsequently turns south bringing a link to the ephemeral drainage course. The loop is completed through the power line right-of-way.

Local pathways identified within the parks are to be programed and themed as per the landscape concept plans. The open space system will be evocative of a prairie watershed by incorporating water elements among prairie grasses, shrubs and trees. It is intended that educational materials be placed to inform the public of the importance and functioning of watersheds.

### Relevant Policies:

*Municipal Development Plan: Parks and open space should accommodate a range of active and passive uses through a variety of facilities that are both flexible and adaptable to the future needs of the city's growing population, such as field houses, community buildings, multi-purpose courts, playgrounds, gathering spaces, open play space, and trails.*

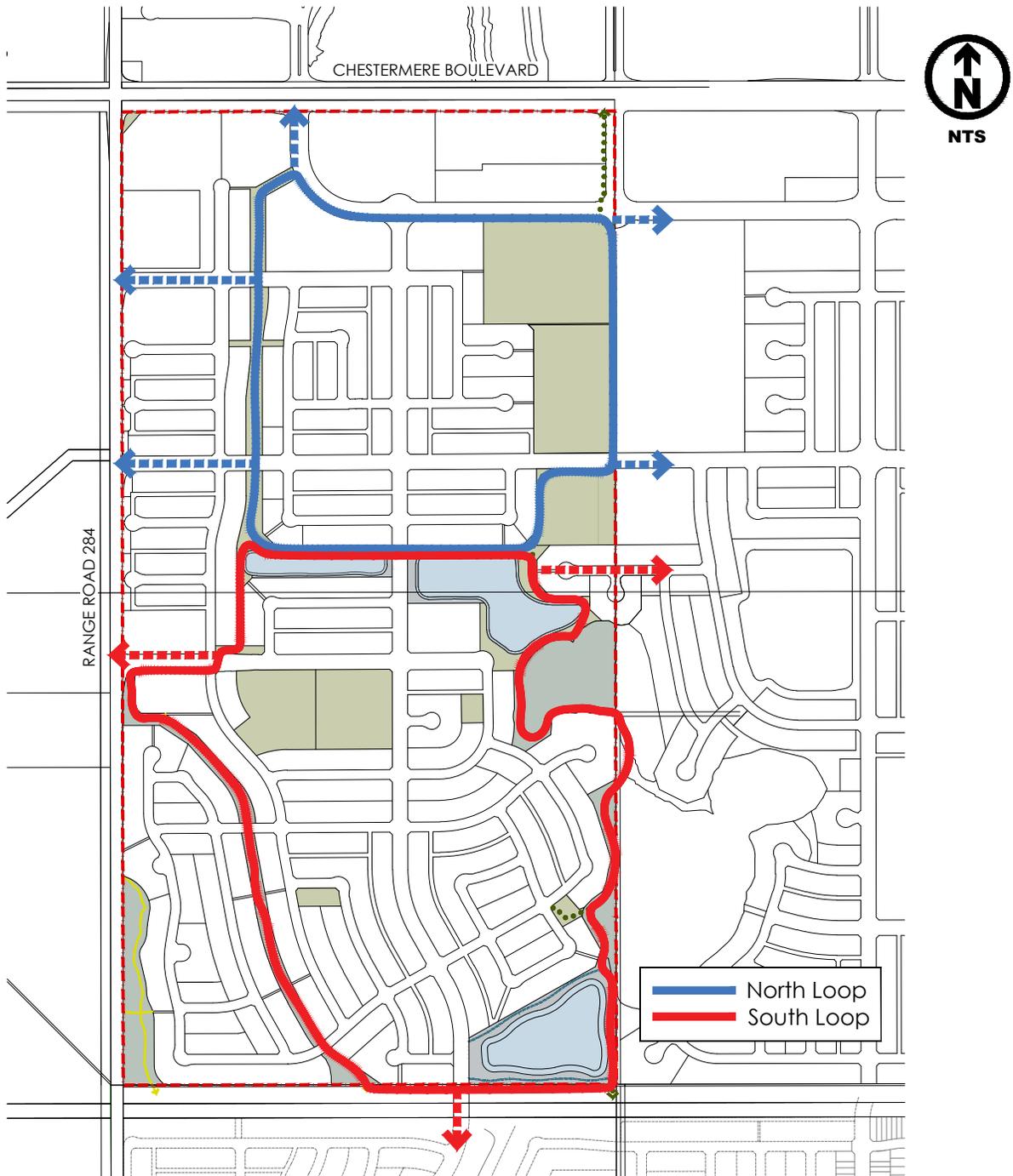
*Waterbridge Master Area Structure Plan: Waterbridge neighbourhoods will be defined by the eastwest arterial grid road network and the north-south central open space corridor created by linear parks, stormwater canals, natural areas and school sites that intersect and connect the four communities.*

*Gateway Area Structure Plan: The Plan Area will contain a variety of parkspace that will be distributed throughout the communities. This park space will take the form of neighbourhood parks, sub-neighbourhood parks and a variety of naturalized open spaces that accommodate canals, pathways and natural areas.*

Figure 13a  
Open Space Plan



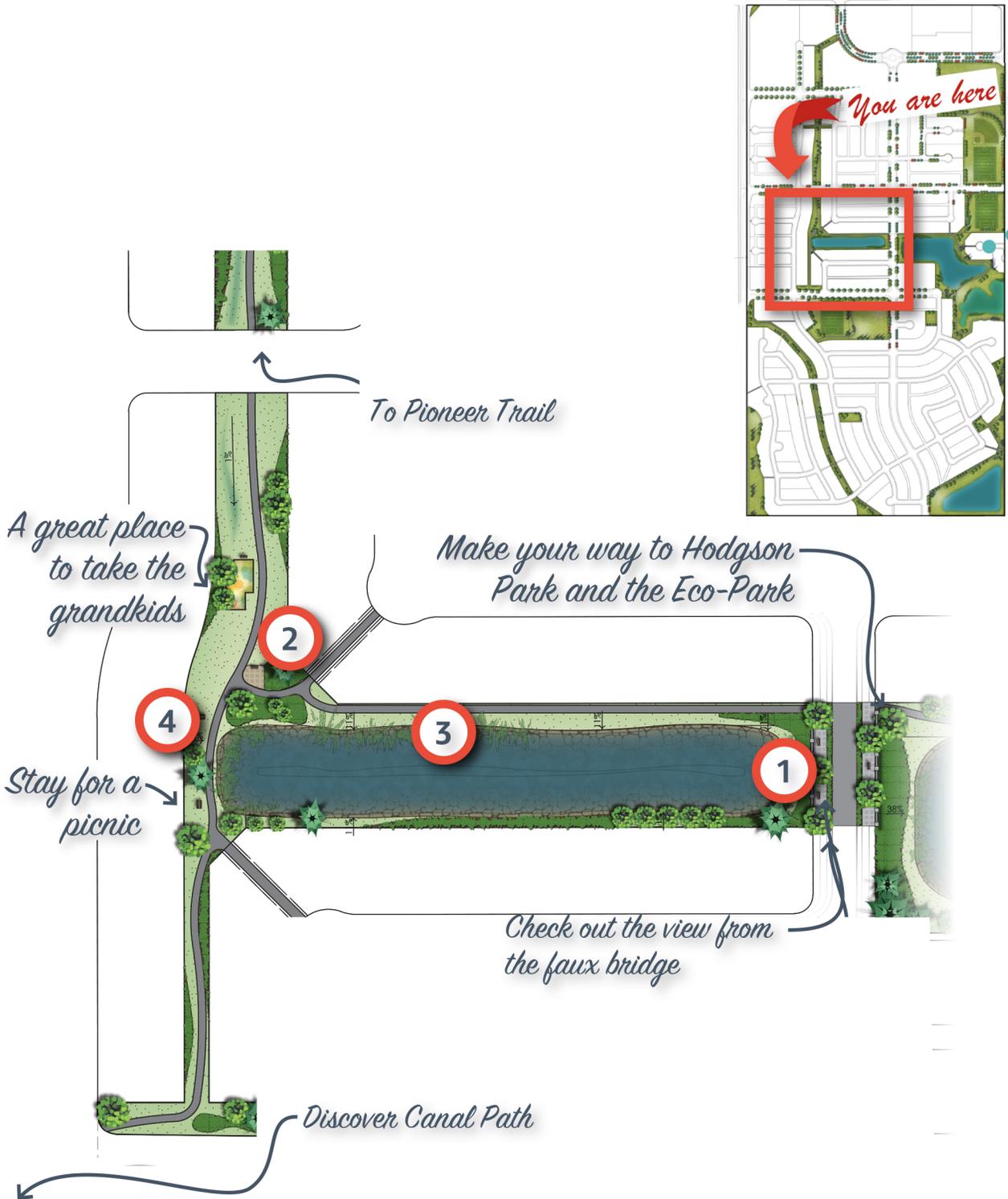
Figure 13b  
Green Loops



Legend

----- Chelsea OP boundary

Figure 13c  
West Hodgson Park Concept



## WEST HODGSON PARK:

West Hodgson Park is where many pedestrian "loops," come together, with pathways converging from all directions. You'll find vistas, ecological and fun activities for everyone here.

1

Faux bridge overlooking the stormpond



2

Outdoor activities along the path



*Connect with old friends and make new ones*



3

Native aquatic plantings along pond edge



4

Playground for all ages and abilities



*All paths lead to Hodgson Park*



Figure 13d  
 East Hodgson Park and Eco Park Concept



Community activities at the Plaza



1

## EAST HODGSON PARK:

East Hodgson Park is all about community events. It features an urban plaza that is perfect for markets, large family gatherings and public events. It also provides the entrance to the Eco Park

Water-based activities throughout the Eco Park



2

*Learn about how water connects us all*

Boardwalks and wetland viewing at key locations



3

## ECO PARK

The Eco Park is an important part of the urban waterscape. An existing wetland, it is home to myriad species of plants and animals, including a thriving bird population. Interpretive signs, lookouts, boardwalks and platforms will help you experience its thriving biodiversity. It's the perfect place to learn, teach, observe and be a part of the natural world.

Interpretive Signage throughout

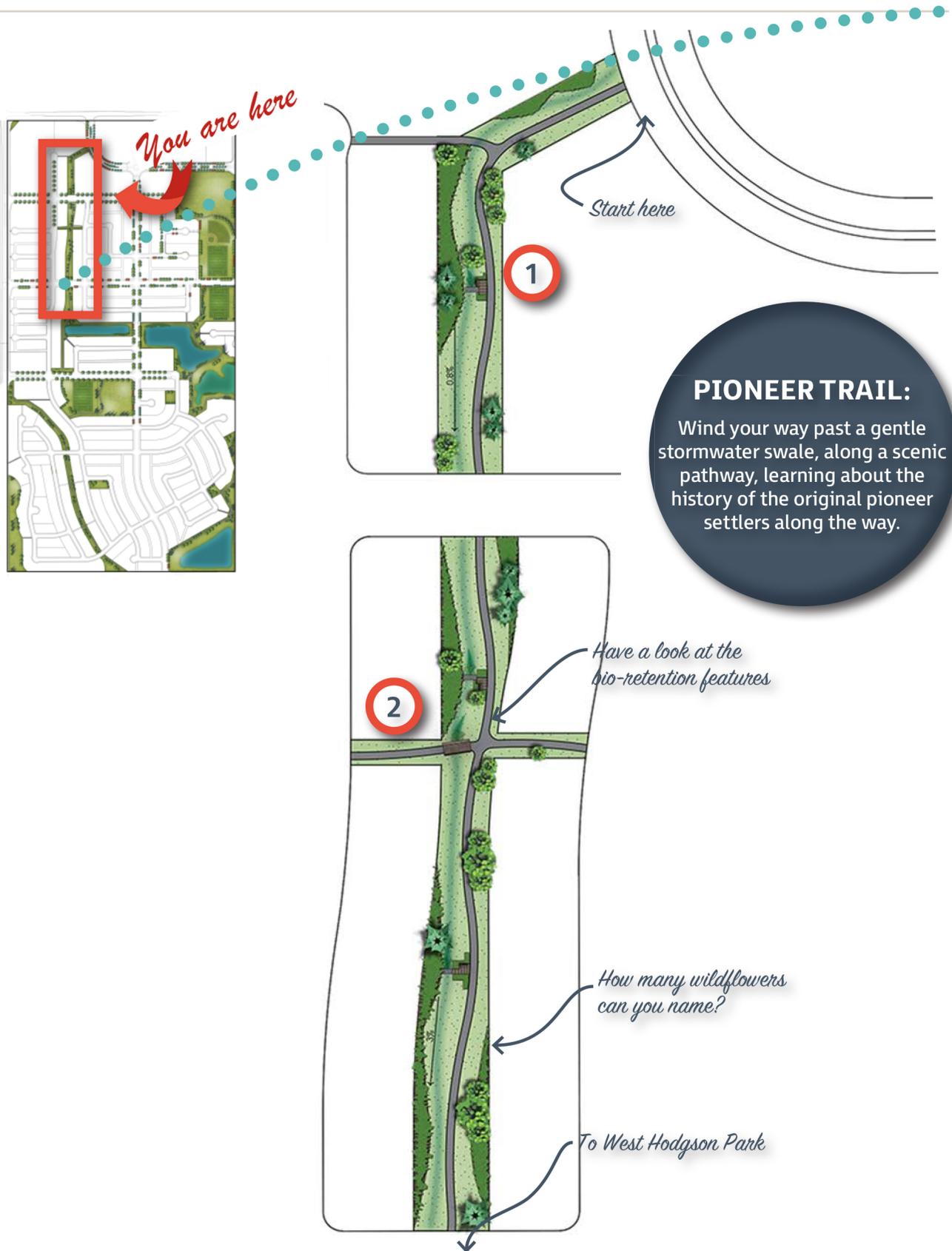


4

5

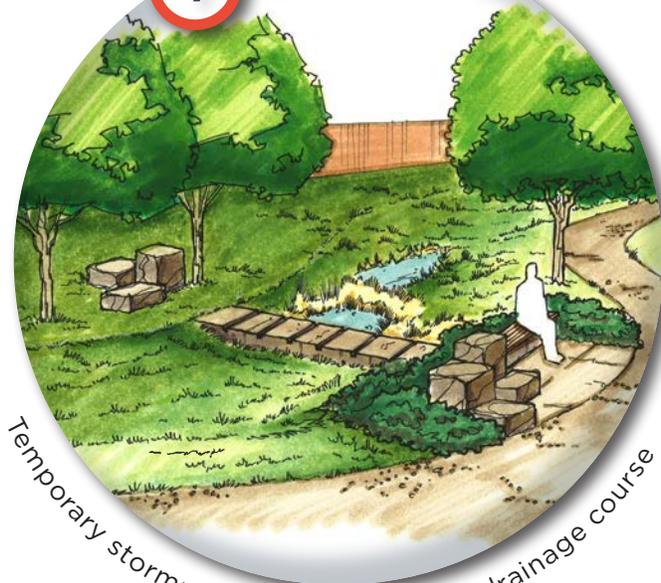


Figure 13e  
Pioneer Trail Park Concept



**PIONEER TRAIL:**  
Wind your way past a gentle stormwater swale, along a scenic pathway, learning about the history of the original pioneer settlers along the way.

1



Temporary stormwater retention along drainage course



*See how water in the environment connects us all*



*Get a taste of the foothills in your own backyard*



*Watch birds in their natural habitat*

2



Natural processes integrated with site furnishings and wayfinding

Figure 13f  
Canal Path Parks Concept



1

Native plants throughout



*Water flowing from the foothills plays a vital role in our outdoor landscape*



A winding informal pathway along the drainage course

2



Naturalized banks and dense plantings, with seating throughout

3

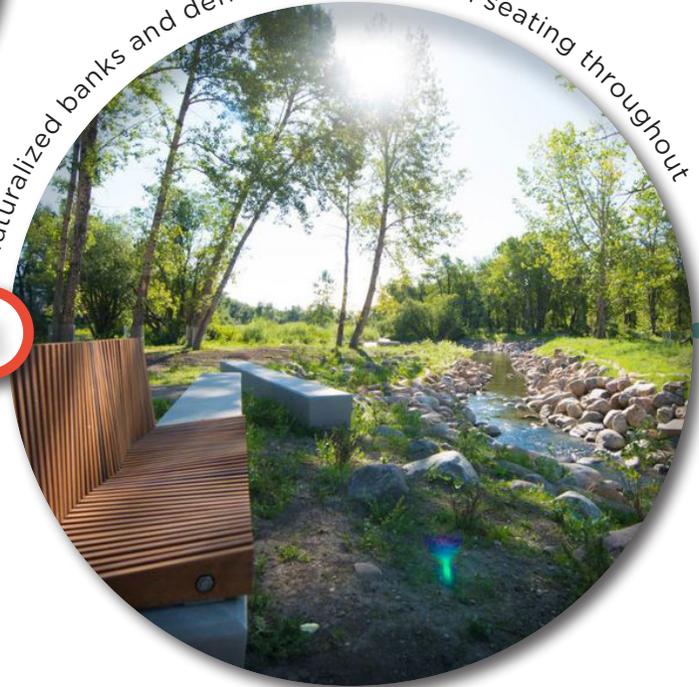


Figure 13g  
Eco Park and Stormpond Concept



Wildlife viewing platforms throughout the Eco Park

1



### ECO PARK & STORMPOND

The Eco Park connects to the stormpond, creating the ideal place to go for a jog, a ride, or a stroll. Once you make your way past the rich biodiversity of the Eco Park, the pathway opens up to give you wide open views of the stormpond and its surrounding parkland.

*The perfect place to stop and rest for a while*



2

Playgrounds and play spots throughout the greenspace system



3

Pathways that wind through the Eco Park and around the Stormpond



Figure 13h  
The Woods and The Green Park Concept



Opportunities to stop and play along the path

1



## THE WOODS:

The existing stand of Aspen trees will remain largely untouched, and features a low-key, granular path. The path helps connect the community to the natural landscape, offering a unique route for your morning walk.

Tree-lined trails amongst the Aspens

2



## THE GREEN:

This small park is the perfect place to meet other families for a get-together, and an excellent landmark for older kids to join up at before walking to school together. It's like a great big front yard for the community.

picnic tables and benches in the park

3



## 6.6 High School Site

The Rocky View School Division has identified a need for a future high school within the Waterbridge MASP. This High School is proposed south of the General Commercial Area along Chelsea Boulevard. The High School parcel will accommodate the school building as well as a supporting playfields. A private road will offer direct access to the school from Chelsea Boulevard as well as public sidewalks and a regional pathway that runs along the east boundary of the parcel. The High School is to complement the proposed Recreation Site to the east on the adjacent property. The High School sportsfields are anticipated to be utilized as a Joint Use Site to permit community sports programming outside of school hours.

### Relevant Policies:

**Municipal Development Plan:** *The City shall collaborate with school authorities on locating joint school and municipal facilities, and developing joint use agreements for these facilities.*

**Waterbridge Master Area Structure Plan:** *The purpose of the school sites is to accommodate public and separate schools with associated playing fields required to serve the educational and recreational needs of the community.*

**Gateway Area Structure Plan:** *The high school and recreational centre site should be located and planned on an integrated basis at the south end of the Town Centre.*

### SCHOOL SITE:

Site of a future high-school, this is where life happens for our up-and-coming residents. With extensive playing fields and wide open spaces, this site has space to grow.



Figure 14  
High School Concept



## 6.7 School Sites

Two school parcels are proposed:

- Site 1: The first school site is located at the intersection of Chelsea Parade and Chelsea Drive in the central-southern as part of the Neighbourhood Node (see Figure 15). This site will include a school building and supporting playfields and is intended for the public school board.
- Site 2: The second school site is located along Chelsea Avenue south of the High School site in the north-eastern portion of the development (see Figure 16) and straddles the boundary between United's and Westcreek's lands. This school site is intended for the separate board. The location of the school building and playfields on the school site will be determined by the school board.

Similar to the High School, school playfields are intended as Joint Use Sites to permit the community to utilize the sportsfields outside of school hours.

### SCHOOL SITE:

Tucked into a quiet residential area, this site is the future home of an elementary school. It's a place to learn to ride a two-wheeler all by yourself, a place to learn the rules of the game, and a place to grow new friendships

### Relevant Policies:

**Municipal Development Plan:** *The City shall collaborate with school authorities on locating joint school and municipal facilities, and developing joint use agreements for these facilities*

**Waterbridge Master Area Structure Plan:** *The purpose of the school sites is to accommodate public and separate schools with associated playing fields required to serve the educational and recreational needs of the community.*

**Gateway Area Structure Plan :** *Public and separate school sites will be located within the Plan Area to meet the educational and recreational needs of the residents.*



Figure 15  
School Site 1 Concept



*Think of this park as an extension of your backyard: a place to play, to learn and to spread your wings*



*Endless grass for kicking about and learning together*

Figure 16  
School Site 2 Concept



*School Site 2*



## 6.8 Eco Park

The western portion of the Major Wetland located along the eastern boundary of the site is to be preserved as an Eco Park and thus conserve wetland habitat and provide a natural amenity for the community and the surrounding region. The Regional Pathway is to run along the western portion of the Eco Park to provide access to the area. In combination with the park and storm pond to the northwest, the Eco Park will serve as an attractive termination for the neighbourhood node.

### Relevant Policies:

**Municipal Development Plan:** *The City shall require appropriate development setbacks around wetlands with high ecological value through mechanisms such as environmental reserve dedication and/or registration of environmental reserve easements.*

**Waterbridge Master Area Structure Plan:** *These waterbodies will perform both a stormwater management and recreational function. They will also provide for a wildlife habitat and recreational activity focus for residents in the area, and an amenity for residential homes backing onto the wetland area.*

**Gateway Area Structure Plan:** *The major wetland complex within Community C will be preserved and enhanced as an Eco Park which will provide a complimentary stormwater and a recreational function, in addition to providing for habitat preservation.*



## 6.9 Range Road 284 Interface

Current planning policy documents and transportation studies identify Range Road 284 as a future four lane arterial road. Nevertheless, the City of Chestermere is in discussions with the City of Calgary regarding the possibility to limit the upgrade of Range Road 284 to a two lane collector to accommodate an attractive and comfortable pedestrian environment. Considering the uncertainty regarding the status of Range Road 284, the Outline Plan has been laid out to accommodate either outcome. Specifically, dwellings along Range Road 284, between Chelsea Parade and Chestermere Boulevard, can be orientated towards Range Road 284 or away from it depending on its standard at the time of development. In addition, buildings will be situated at corners of the intersections along Range Road 284 to mark the gateways into the community. A demonstration of the possible interface along this section of Range Road 284 is shown in the following figures.

Decorative fencing will be placed along the Range Road south of Chelsea Parade to enhance the appearance of the corridor and to accommodate a varied street environment. The decorative fence line will terminate at the preserved tree stand situated at the southern boundary of the site. The backyard fencing along Range Road 284 is designed to provide visual interest, a sense of rhythm, and placemaking to the roadway. It is designed to reference beachfront wood fences, with weathered wood texture on the concrete sections, and transparent panels that incorporate the shapes of beach grasses. Plantings in the recesses formed by the gaps in the concrete panels would be either flowering vines or perennial grasses. An alternative, smoother-faced concrete version is also presented as a clean, modern concept.



Figure 17a  
Range Road 284 Possible Interface





Figure 17b  
Range Road 284 Fencing Concepts





## 6.10 Chestermere Boulevard Interface

The Chestermere Boulevard Corridor Plan anticipates buildings fronting the portion of Chestermere Boulevard north of the subject site. In consideration, the proposed semi-detached and townhouse units will implement design elements on facades fronting onto Chestermere Boulevard. Proposed apartments and commercial buildings will include *four sided* architecture. Vehicles, sidewalks, bicycle lanes and transit leading from Chelsea can be integrated with the future alignment of Chestermere Boulevard anticipated by the Corridor Plan.

Figure 18  
**Chestermere Boulevard Interface**



## 6.11 Regional Pathway

A Regional Pathway will traverse the site in a north-south direction through the eastern portion of the site. The Regional Pathway will connect the residents with various community elements including the General Commercial Area, the High School, parks, the Neighbourhood Node and the Eco Park. The pathway will also connect Chelsea with the future adjacent communities to the east and south.

### Relevant Policies:

**Waterbridge Master Area Structure Plan:** *Regional pathways will be aligned along the north/south open space corridor that extends through the area. Connecting to the regional pathway will be a system of local pathways that allow opportunities for walking around wetlands and watercourses, and within the neighbourhoods and activity centres.*

**Gateway Area Structure Plan:** *Regional pathways will connect to local pathways, school sites, major wetlands and the town centre. Notably, the regional pathways will be aligned along a north/south corridor that extends through the communities.*

Figure 19  
Regional Pathway



#### LEGEND

●●●●● REGIONAL PATHWAY

## 6.12 Built Form & Housing Diversity

Located on the western edge of Chestermere, Chelsea places an added value on family, recreation and the great outdoors and offers a variety of housing choice for its residents. With an extensive open space network connecting residents to nearby shopping and the Eco Park, convenient access to community amenities and Chestermere Lake just moments away, the community of Chelsea will provide residents a charming and peaceful quality of life 365 days a year.

Inspired by family-focused resort havens, coastal towns and nature's biggest playgrounds, every facet of Chelsea's development will endeavour to feel distinctive, yet comfortable in its surroundings.

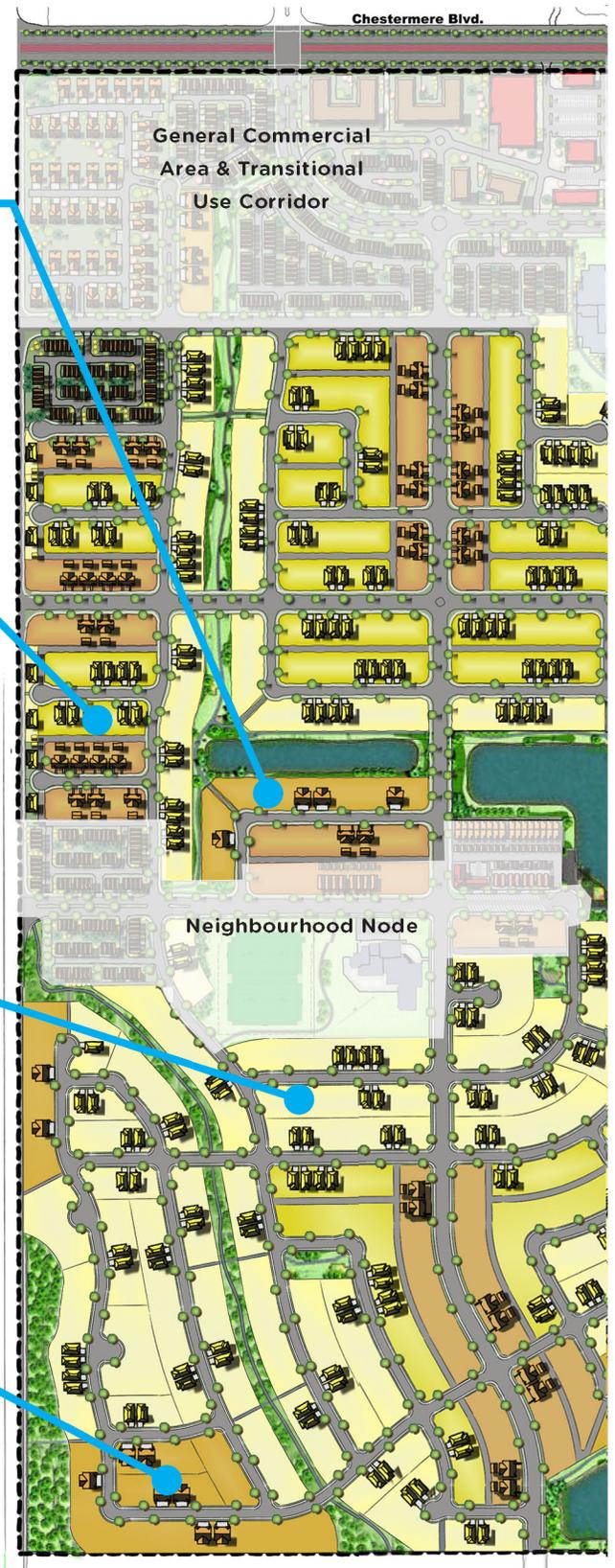
Architectural design will emphasize natural features that complement the natural landscape. With choice and diversity in architectural styles and lot types, homes will reflect the beauty of the surroundings with a coastal inspiration.

To achieve a distinctive coastal look that harkens back to family-friendly resort towns, a number of architectural elements can be incorporated:

- The use of vertical board and batten;
- Front facing gables and dormers;
- The use of shutters, vents and louvers;
- Six-inch corner boards and prominent window trim;
- A variety of exterior cladding material such as, horizontal and vertical siding, masonry, shakes, and decorative paneling; and
- "Carriage Style" garage doors.

The following figures provide an illustration of the architectural theming and massing envisioned for the future community of Chelsea. Imagery is presented to demonstrate the anticipated building types for each land use district and area of the development.

Figure 20a  
Built Form & Housing Diversity — Residential Area



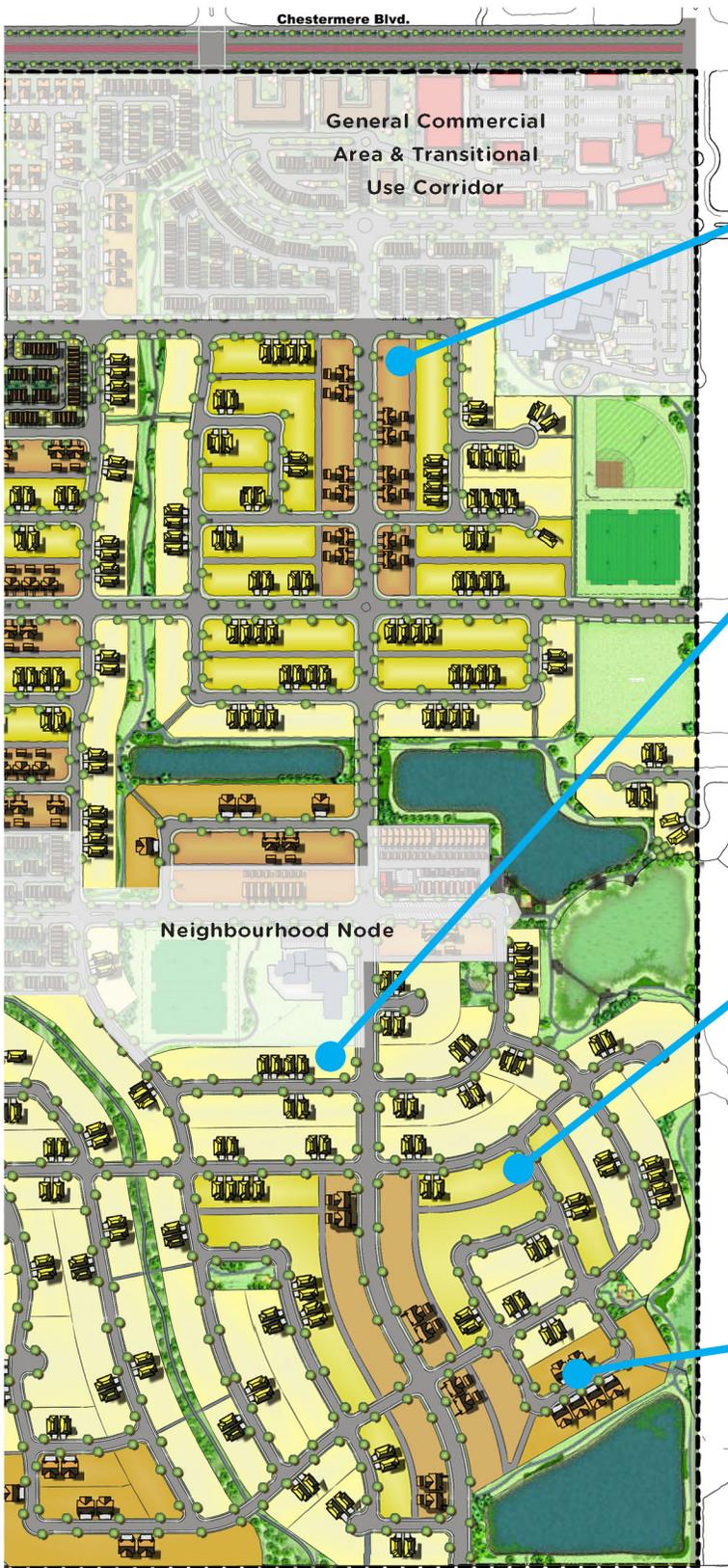
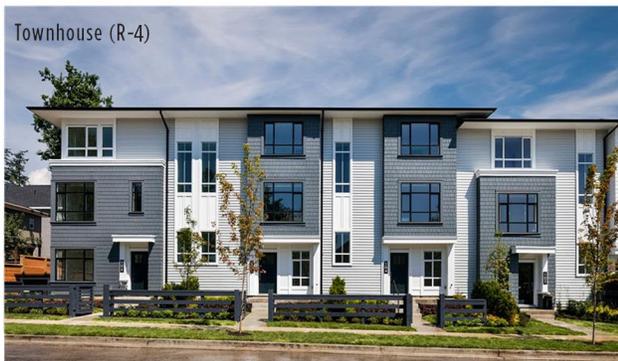
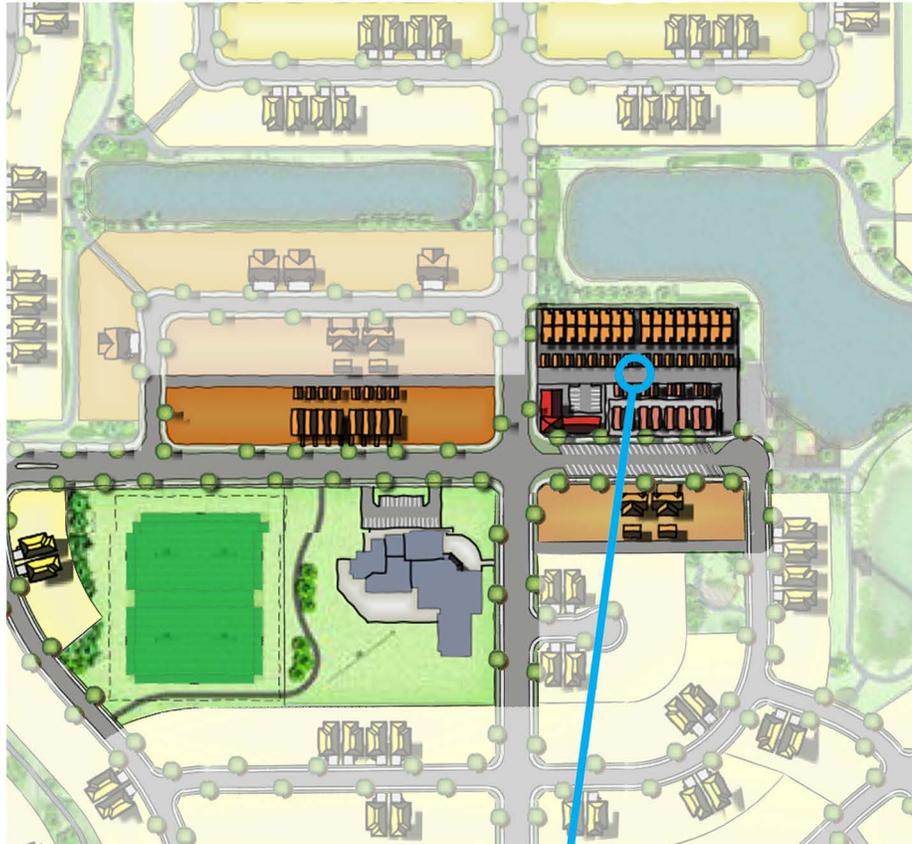


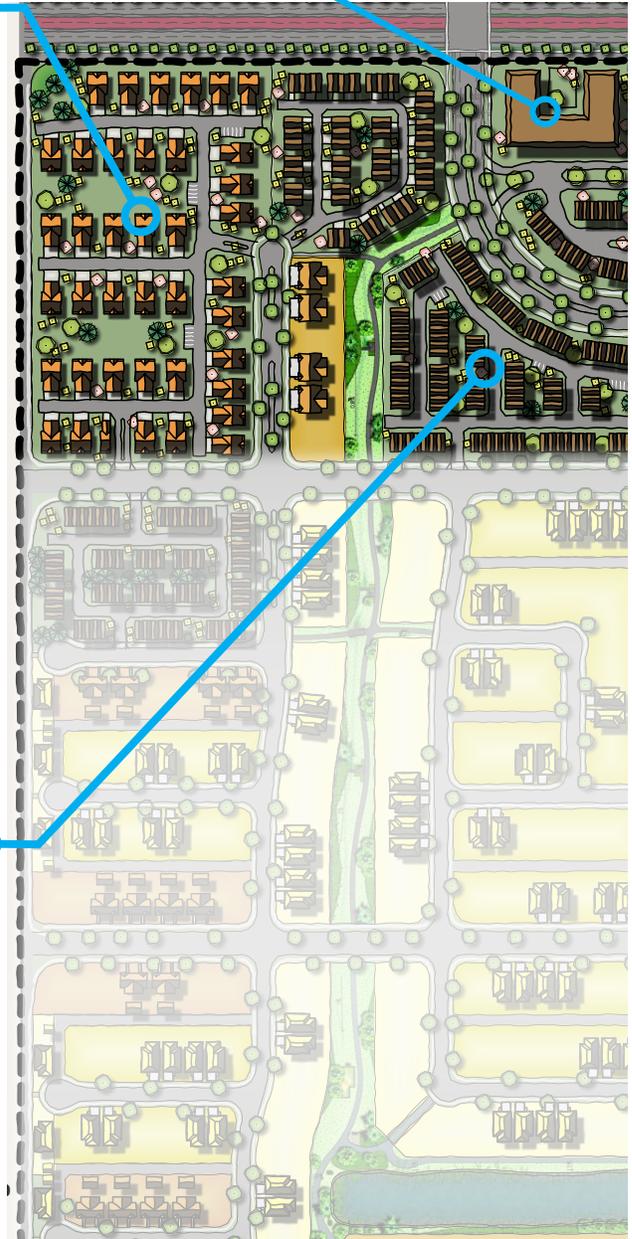
Figure 20b  
Built Form & Housing Diversity — Neighbourhood Node





Note: Conceptual and subject to change

Figure 20c  
Built Form & Housing Diversity – General Commercial Area & Transitional Use Corridor



General Commercial Area (C)



General Commercial Area (C)



General Commercial Area (C)



General Commercial Area (C)



## Section 7.0

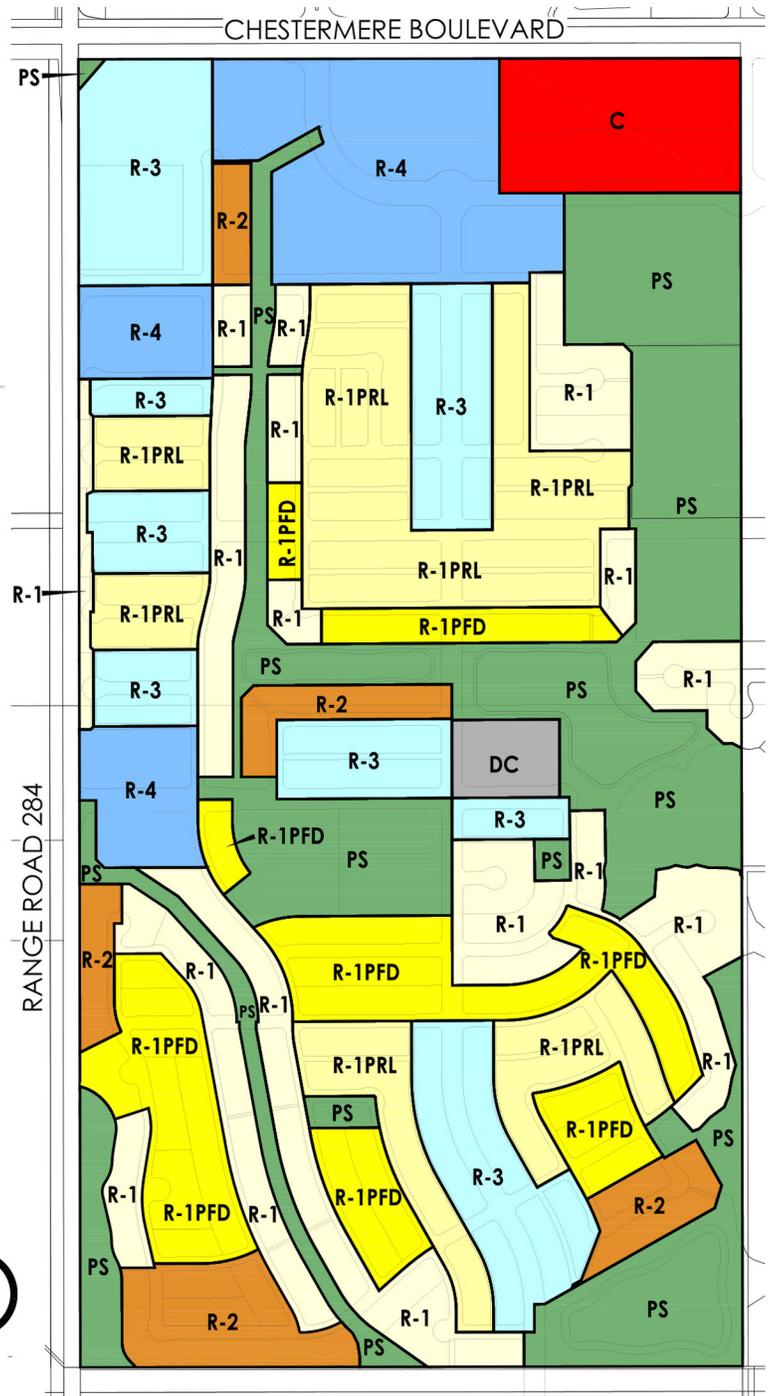
# Land Use Districts

The following land uses districts are proposed as shown in Figure 21: Land Use Plan for the subject site to implement the envisioned development.

Figure 21  
Land Use Plan

Land Use Redesignation Statistics		
UT to R-1	20.83 ha	51.48 ac
UT to R-1PFD	14.52 ha	35.89 ac
UT to R-1PRL	17.71 ha	43.76 ac
UT to R-2	7.20 ha	17.79 ac
UT to R-3	17.75 ha	43.85 ac
UT to R-4	13.30 ha	32.86 ac
UT to DC	1.24 ha	3.06 ac
UT to C	4.80 ha	11.86 ac
UT to PS	30.69 ha	75.84 ac
<b>Total Redesignation</b>	<b>128.04 ha</b>	<b>316.39 ac</b>

	R-1	Residential Single Detached
	R-1PFD	Residential Planned Lot Front Drive
	R-1PRL	Residential Planned Lot Rear Drive
	R-2	Residential Semi-Detached
	R-3	Residential Multi-Unit
	R-4	Residential Low Rise Multi-Unit
	DC	Direct Control
	C	Local Commercial
	PS	Public Service





## 7.1 Residential Single Detached (R-1) District

The significant portion of the site is proposed to be redesignated to Residential Single Detached (R-1) district. The purpose of this District is to provide for low density residential development in the form of single-detached dwellings that have high standards of design and appearance.

## 7.2 Residential Single Detached Planned Lot (R-1P) District

The Planned Lot District is intended to provide a narrower single family lot within a comprehensively planned block to ensure high quality streetscapes through strict District guidelines. This land use district is applied throughout Chelsea to offer a different housing opportunity in a unique-to-Chestermere manner. Planned lots are not to compromise more than 50% of the single-family housing within an outline plan.

## 7.3 Residential Semi Detached (R-2) District

The Residential Semi Detached (R-2) district is located to provide a mix of low profile units in strategic locations throughout the Plan Area. The purpose of this District is to provide for residential neighbourhoods which have a high standard of design and appearance. Semi-detached, duplex and single detached dwelling are allowed in the district.

## 7.4 Residential Multi-Unit (R-3) District

Residential Multi-Unit (R-3) District is distributed mostly throughout the northern portion of the community to provide a mix of units. The purpose of this District is to provide for multi-unit residential development, along with semi-detached development in order to increase the variety of dwelling types available in the community. Unit row types allowed within the district include duplex dwellings, semi-detached, stacked townhouses and townhouses.

## 7.5 Low Rise Multi-Unit Residential (R-4) District

Six portions of the subject site are identified for the Low Rise Multi-Unit Residential (R-4) district. The two main locations proposed for this district are within the Neighbourhood Node and Transitional Use Corridor. Unit types allowed within the district include row/townhouses, stacked townhouses and apartment buildings. It is also noted that this district allows for home occupations to support local employment.

The R-4 parcel located on the south side of Chelsea Parade is envisioned to accommodate townhouse development with maximum density of 24 upa, providing a better interface with the nearby single detached (R-1) homes.

## 7.6 Local Commercial (C) District

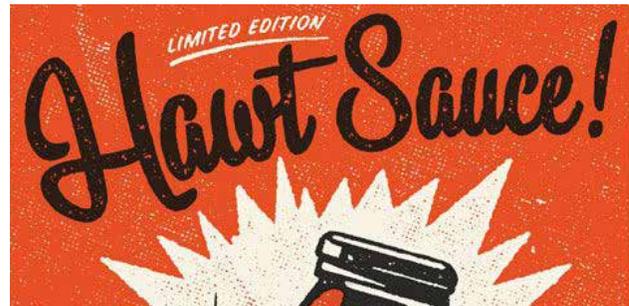
It is proposed that the Regional Commercial area be redesignated to the Local Commercial (C) District. The purpose of this District is to provide for a wide range of commercial uses, encourage a high quality, aesthetically pleasing pedestrian environment and encourage a high architectural design standard for buildings. A variety of commercial uses are allowed within the Local Commercial district including but not limited to retail, personal services, restaurants, offices and child care facilities.

## 7.7 Direct Control Mixed Use (MU) District

A portion of the Neighbourhood Node is proposed to be redesignated to the Direct Control Mixed use (MU) District. The purpose of this District is to provide flexibility to comprehensively develop an integrated mixed use site accommodating commercial and residential uses.

## 7.8 Public Service (PS) District

It is proposed that the municipal reserve, municipal school reserve, environment reserve and Public Utility Lot components of development be redesignated to the Public Service (PS) district. The purpose of this District is to accommodate recreational and educational facilities, places of worship, offices, health and research facilities and public utilities to service the community.



# Section 8.0 Outline Plan



## 8.1 Outline Plan Statistics

Outline Plan Statistics				
<b>TOTAL AREA OUTLINED</b>	<b>128.04 ha</b>	<b>316.39 ac</b>		
 less PS - Environmental Reserve	6.35 ha	15.69 ac		
<b>GROSS DEVELOPABLE AREA (GDA)</b>	<b>121.69 ha</b>	<b>300.70 ac</b>		<b>100.0%</b>
			Frontage	Units
<b>RESIDENTIAL</b>	<b>67.85 ha</b>	<b>167.68 ac</b>		<b>55.8%</b>
<b>Low Density (Single Family)</b>	<b>52.40 ha</b>	<b>129.50 ac</b>		<b>43.1%</b>
 <b>R1</b>	16.06 ha	39.68 ac	4235m	308 units
Anticipated Number of Units (13.75m lot width)				
 <b>R1-PFD</b>	10.60 ha	26.19 ac	2897m	264 units
Anticipated Number of Units (10.97m lot width)				
 <b>R1-PRL</b>	11.83 ha	29.24 ac	3424m	374 units
Anticipated Number of Units (9.14m lot width)				
 <b>R-2</b>	5.60 ha	13.84 ac	1508m	150 units
Anticipated Number of Units (10.0m lot width)				
 <b>R-3</b>	5.63 ha	13.91 ac	1668m	273 units
Anticipated Number of Units (6.1m lot width)				
 <b>R-3 Laned Duplex</b>	2.69 ha	6.64 ac	772m	102 units
Anticipated Number of Units (7.52m lot width)				
Total frontage			14,504m	
<b>SUB-TOTAL UNITS (SINGLE FAMILY)</b>			<b>Anticipated</b>	<b>1471 units</b>
<b>Multi-Residential / Mixed-Use</b>	<b>15.45 ha</b>	<b>38.18 ac</b>		<b>12.7%</b>
 <b>R-3</b>	4.03 ha	9.97 ac		100 units
Anticipated number of units (10 upac)				
 <b>R-4</b>	6.79 ha	16.79 ac		336 units
Anticipated number of units (20 upac)				
 <b>R-4</b>	0.80 ha	1.97 ac		47 units
Anticipated number of units (24 upac)				
 <b>R-4</b>	2.87 ha	7.09 ac		284 units
Anticipated number of units (40 upac)				
 <b>DC</b>	0.96 ha	2.36 ac		94 units
Anticipated number of units (40 upac)				
<b>TOTAL UNITS</b>			<b>Anticipated</b>	<b>2332 units</b>
<b>DENSITY</b>			<b>Anticipated: 21.0 upha</b>	<b>8.5 upac</b>
<i>Regional Commercial &amp; High School Site removed from density calculation to provide Gross Residential Area: 111.15 ha (274.65 ac)</i>				
<b>COMMERCIAL</b>	<b>4.12 ha</b>	<b>10.19 ac</b>		<b>3.4%</b>
 <b>C - Regional Commercial</b>	4.12 ha	10.19 ac		
<b>CREDIT OPEN SPACE</b>	<b>12.16 ha</b>	<b>30.05 ac</b>		<b>10.0%</b>
 <b>PS - Park</b>	3.70 ha	9.14 ac		
 <b>PS - School Sites</b>	4.84 ha	11.97 ac		
 <b>PS - High School Site</b>	3.62 ha	8.94 ac		
<b>NON-CREDIT OPEN SPACE</b>	<b>2.80 ha</b>	<b>6.92 ac</b>		<b>2.3%</b>
 <b>PS - High School Site, Playfields (Hatched)</b>	2.80 ha	6.92 ac		
<b>PUBLIC DEDICATION</b>	<b>34.75 ha</b>	<b>85.86 ac</b>		<b>28.6%</b>
 <b>PS - Stormwater Ponds</b>	5.47 ha	13.51 ac		
 <b>PS - PUL</b>	1.54 ha	3.81 ac		
Roads and Lanes	27.74 ha	68.54 ac		



## 8.2 Density

The Chelsea outline plan will provide an anticipated 2,332 residential units in a combination of single, semi-detached, row housing and multi-residential forms. The Gross Residential Area is 111.45 hectares (276.95 acres) which is the total area of the subject site less regional uses including the High School site, the General Commercial Area and the undevelopable environmental reserve parcels. The result is that the density of the Chelsea outline plan area is approximately 21.0 units per hectare (8.5 units per acre). This density calculation follows the methodology of the Calgary Metropolitan Plan. The anticipated population is 6,063 based on Alberta's average household size of 2.6 people per dwelling unit in 2011.

## 8.3 Municipal Reserve Analysis

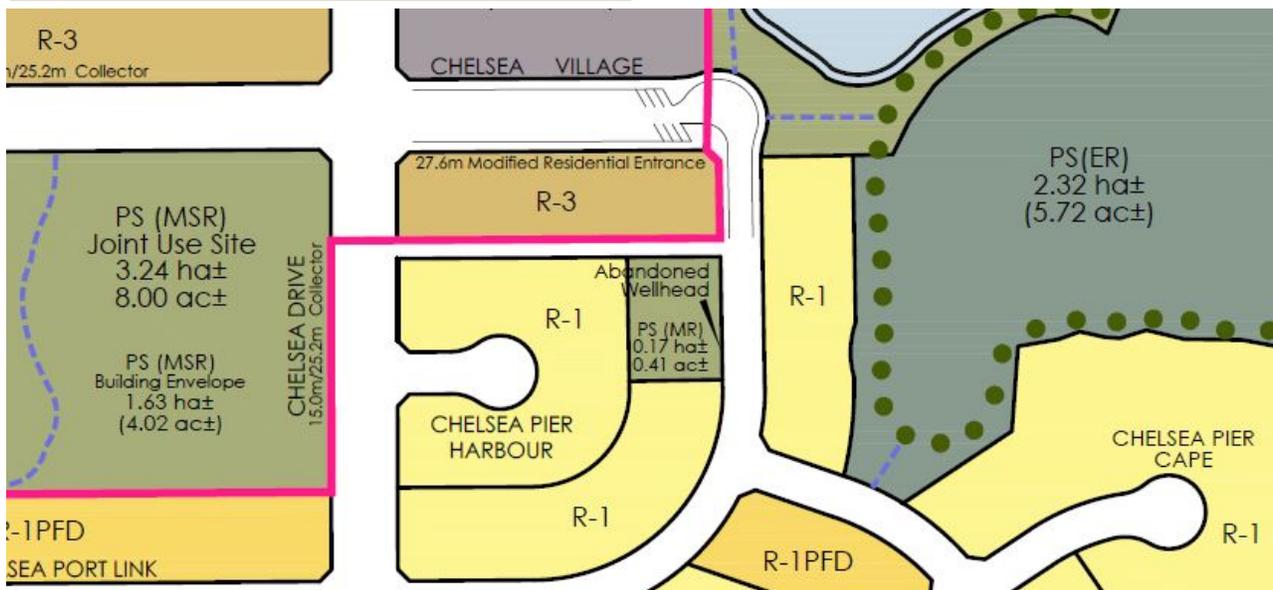
A number of parcels will be dedicated for Municipal Reserve (MR) and Municipal School Reserve (MSR). The MR parcels will accommodate the High School playfields and linear open space. Three MSR parcels will be provided to accommodate the High School, one full school site, a half school site and associated playfields. The plan area provides 12.20 hectares (30.14 acres) or 10.0% of credit open space and 2.80 hectares (6.92 acres) or 2.3% of non-credit open space for a total area of 15.00 hectares (37.07 acres). The process for acquiring the non-credit open space portion of the High School site shall be determined through a negotiated agreement between the City and the landowner. The non-credit MR that accommodates the decommissioned abandoned well was not included in the calculation (see Section 8.4: Decommissioned Abandoned Well).

	Hectares	Acres	Percent
<b>Total Area</b>	<b>128.04</b>	<b>316.39</b>	
Environmental Reserve (ER)	6.35	15.69	
<b>Gross Developable Area</b>	<b>121.69</b>	<b>300.70</b>	<b>100%</b>
<b>Credit Open Space Provided</b>	<b>12.16</b>	<b>30.05</b>	<b>10.0%</b>
Parks, Linear Parks (MR)	3.70	9.14	3.6%
School Sites, Playfields (MSR)	4.84	11.97	3.9%
High School Site, Playfields (MSR/MR)	3.62	8.94	2.5%
<b>Non-Credit Open Space Provided</b>	<b>2.80</b>	<b>6.92</b>	<b>2.3%</b>
High School Site (MSR/MR)	2.80	6.92	2.3%
<b>Total Open Space (Credit and Non-Credit)</b>	<b>14.96</b>	<b>36.97</b>	<b>12.3%</b>

## 8.4 Decommissioned Abandoned Well

In accordance with the requirements of the Government of Alberta, access to the decommissioned abandoned well will be maintained by providing the regulatory setbacks within a non-credit MR parcel.

Figure 23  
Decommissioned Abandoned Well



## 8.5 Neighbourhood Node Density

The Waterbridge MASP, Section 8.2.2.2 provides the following policy with respect to the neighbourhood node:

*A Neighbourhood Node shall contain a minimum of 90 dwelling units on 2.0 hectares (5 acres) of net developable land, which equates to a minimum density of 45 units per net hectare (18 units per net acre).*

In compliance with this policy, the proposed neighbourhood node contains approximately 94 units on 0.96 hectares (2.36 acres) of net developable land, which equates to a minimum density of 97.9 units per net hectare (39.8 units per net acre).

## Section 9.0

# Transportation



## 9.1 Road Network

The overall intent of the transportation system is to enable people to reach their destinations, both locally and to the surrounding areas of Chestermere, as efficiently and safely as possible. Externally, this is accomplished by a transportation network that emphasizes connections to Range Road 284 to the west, Chestermere Boulevard (Highway 1A) to the north and future communities to the east and south. Internally, the transportation network is predominately a modified grid with a limited number of crescents, cu-de-sacs and lanes. The structure of the system is maintained by the following collector roads:

- Chelsea Boulevard (East-West/North-South 30 m Primary Collector);
- Chelsea Road (East-West, 25.2 m Collector Street);
- Chelsea Avenue (East-West, 25.2 m Collector Street);
- Chelsea Parade (East-West, 29 m Primary Collector/25.2 m Collector Street);
- Chelsea Port Crossing (East-West, 21.0 m Modified Collector Street); and
- Chelsea Drive (North-South, 25.2 m Collector Street).

Residential roads branch off from the collector roads to provide access to the dwelling units. The result of this road layout is a Street Connectivity Index of 1.5 which is similar to a typical modified grid network which scores 1.3 - 1.5 and equals the MDP target of 1.4.

The outline plan identifies anticipated street names for the development. At the time of subdivision, proposed street names will be submitted to the City for review. The street names of the roads that traverse more than one subdivision or community will be coordinated between developers who have active outline plan applications. Street names are to be in accordance with the City of Chestermere street naming policies and subject to City approval.



Figure 24a  
Transportation Plan

- Legend
- 30m PRIMARY COLLECTOR
  - 29m PRIMARY COLLECTOR
  - 25.2m COLLECTOR STREET
  - 23.5m RESIDENTIAL ENTRANCE STREET
  - 21.0m COLLECTOR STREET
  - 16m RESIDENTIAL STREET
  - 10m LANE
  - 7m LANE

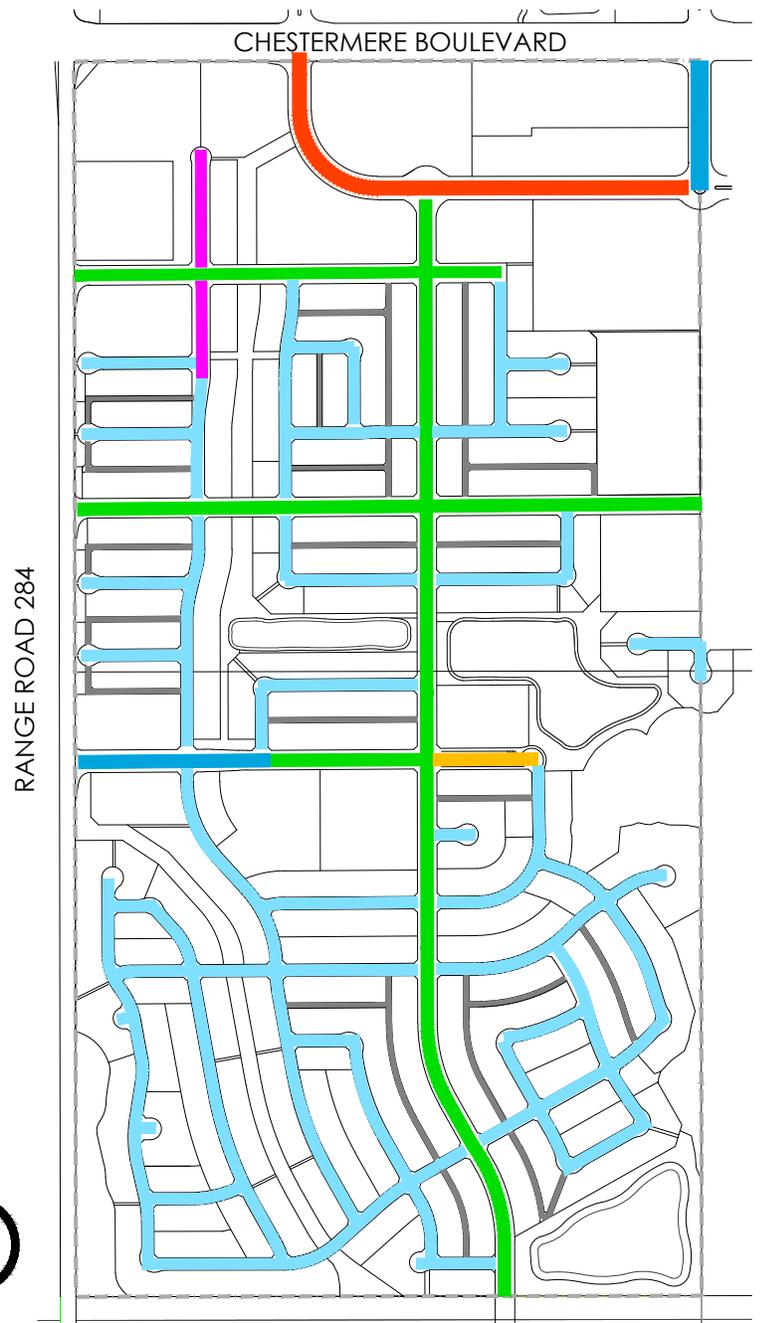
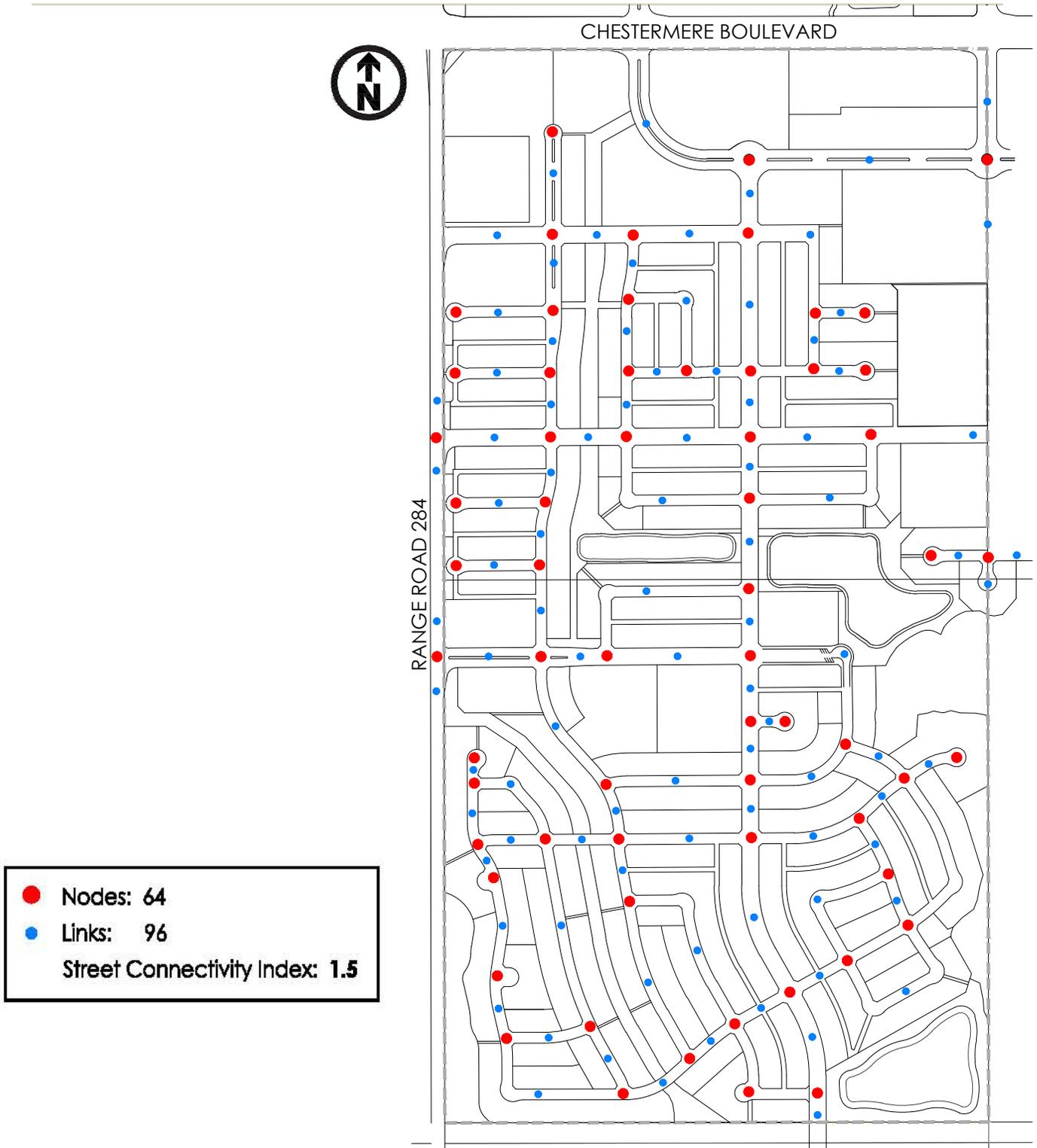


Figure 24b  
Street Connectivity Index



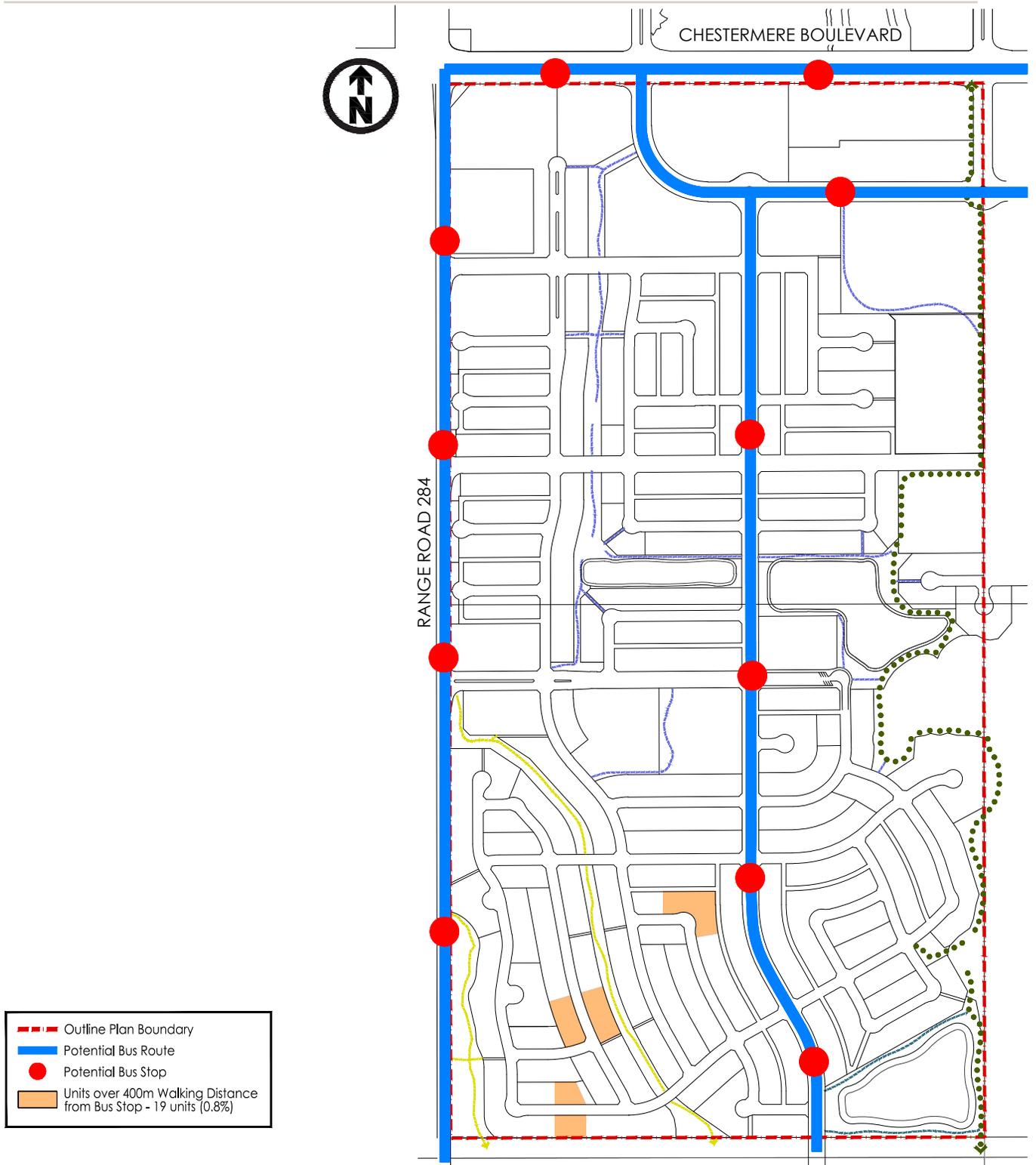
## 9.2 Public Transit Network

Although transit service is not currently provided with Chestermere, Chelsea has been designed to accommodate future transit once available. All transit routes are proposed to be located on Arterial and Collector Standard roads and specifically:

- Range Road 284;
- Chestermere Boulevard (Highway 1A);
- Chelsea Boulevard; and
- Chelsea Drive.

As indicated on Figure 25, the proposed transit stops are distributed evenly and situated near community amenities and main intersections. This will provide all residents with access to public transportation. The typically accepted measure for determining walkability to public transit is to ensure 90% of all residential units are within a 400m (5 minute) walking distance to a transit stop. The site area is designed such that 100% of residential units are within 400m of a transit stop.

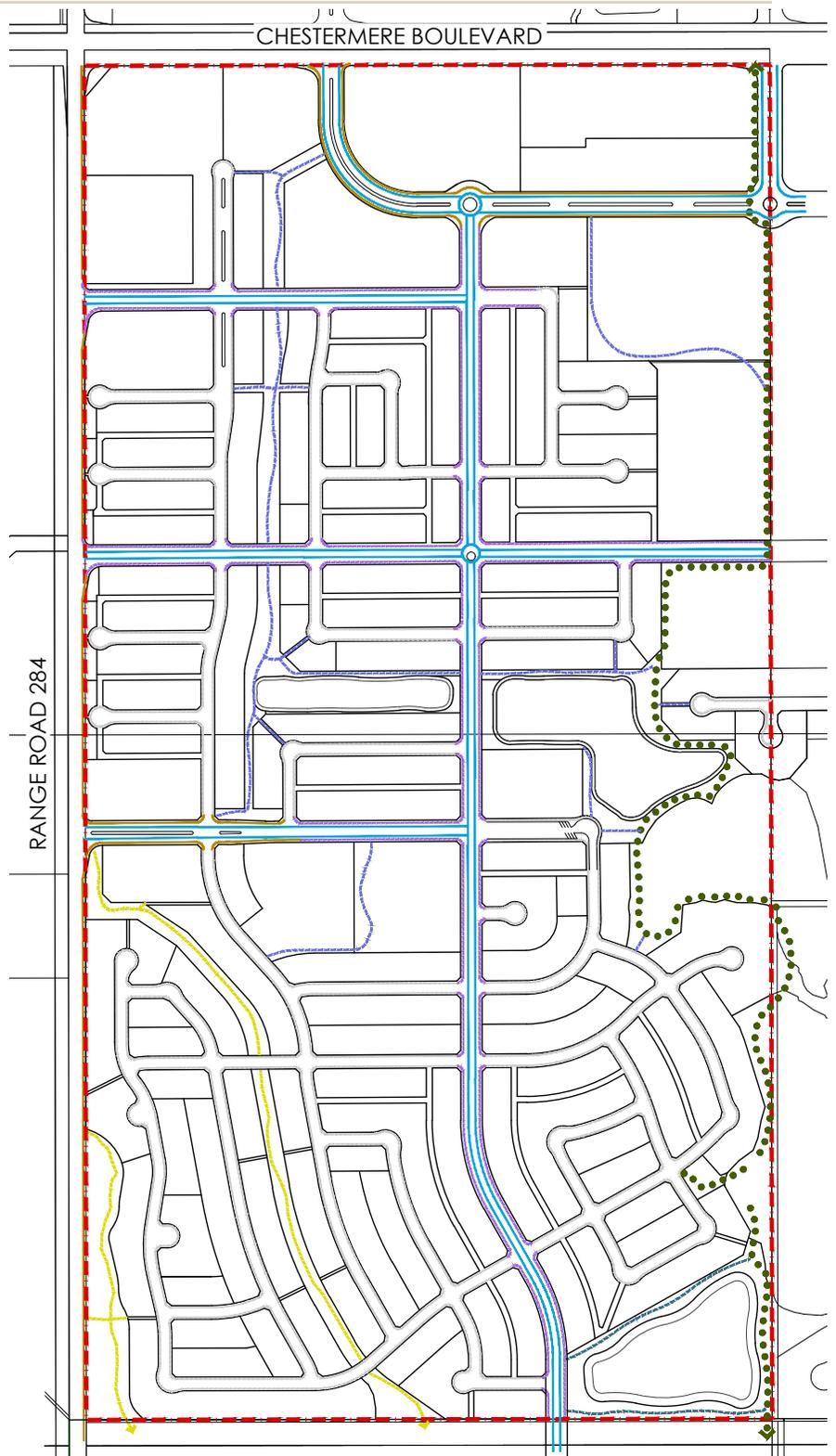
Figure 25  
**Transit Network**



## 9.3 Pedestrian & Cycling Network

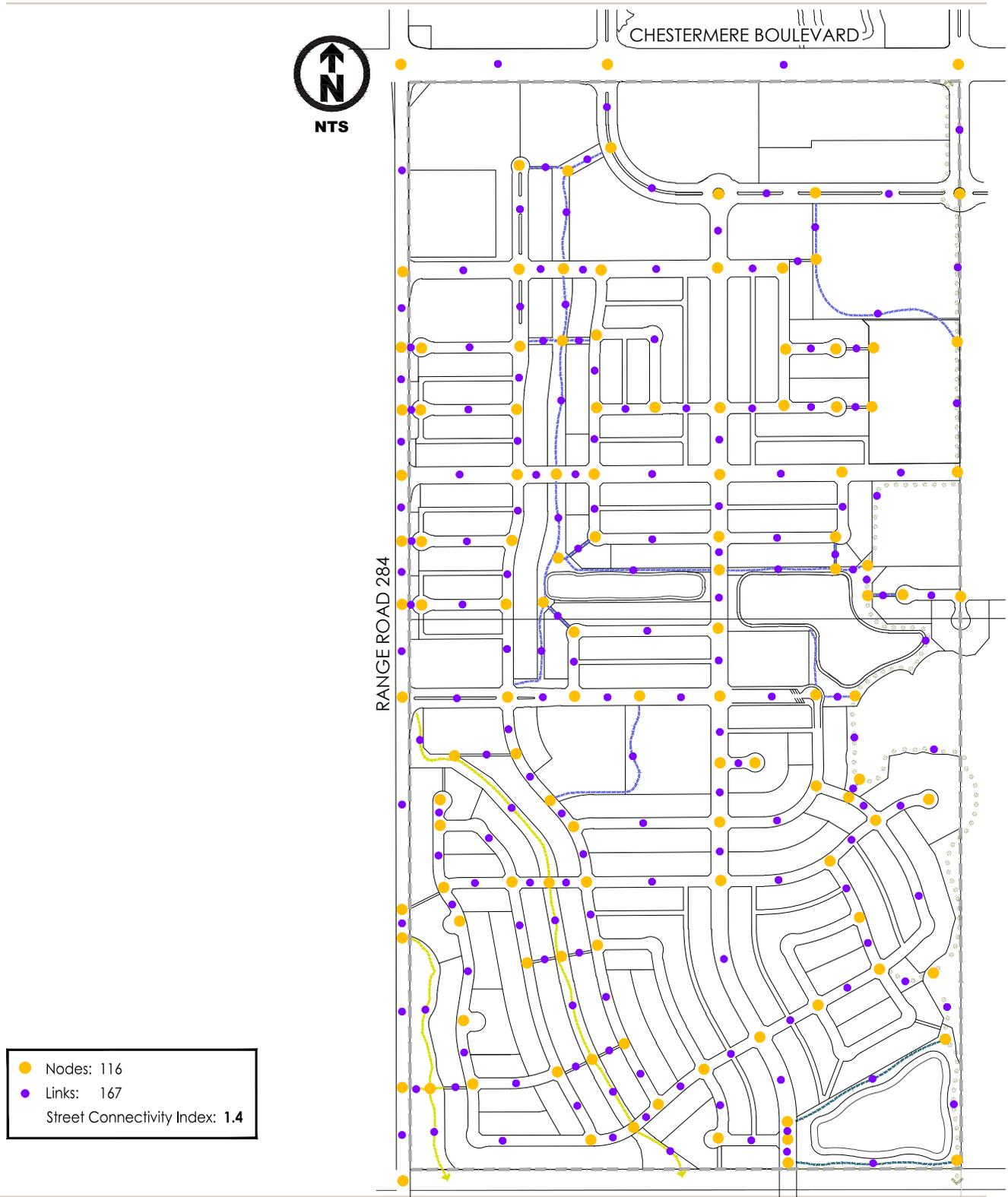
Pedestrian circulation and cycling is facilitated throughout the site by a large network of pathways, trails, sidewalks and bicycle lanes. These pedestrian and cycling routes provide connectivity to all residences, schools, commercial areas and parks. The result is that the community scores an Active Mode Index of 1.4 which is a typical modified grid network score of 1.4 - 1.5 and meets the MDP target of 1.4 for Greenfield Residential.

Figure 26a  
**Pedestrian Network**



- - - - Outline Plan Boundary
- - - - 1.5m Mono Sidewalk
- - - - 2.0m Mono Sidewalk
- 2.0m Separate Sidewalk
- ● ● ● 3.0m Regional Pathway
- - - - 4.0m Maintenance Pathway
- - - - 2.0m Local Pathway
- - - - Trail
- 1.5m Bike Lane

Figure 26b  
Active Modes





## Section 10.0 Servicing



### 10.1 Water Servicing

The majority of the Chelsea lands fall within the City's new North Pressure Zone, which will ultimately be serviced by the proposed Northwest Water Reservoir. Construction of the Northwest Water Reservoir has been tentatively scheduled for completion in 2018. The City and Chestermere Utilities Inc. (CUI) have confirmed that the existing water supply system has excess capacity that can be allocated to new development. Water service for the initial development phases in Chelsea can be provided from this existing excess capacity by extension of watermains from the east side of Rainbow Road westward to the Chelsea lands. The existing spare capacity is part of the City's current (lower) pressure zone; should the initial Chelsea phases fall within the new higher pressure zone service can be provided from the existing City water system grid extension with a temporary pressure booster station on the Chelsea lands. All watermain systems will be looped through contiguous development to provide the required redundancy.



Figure 27a  
Water Servicing

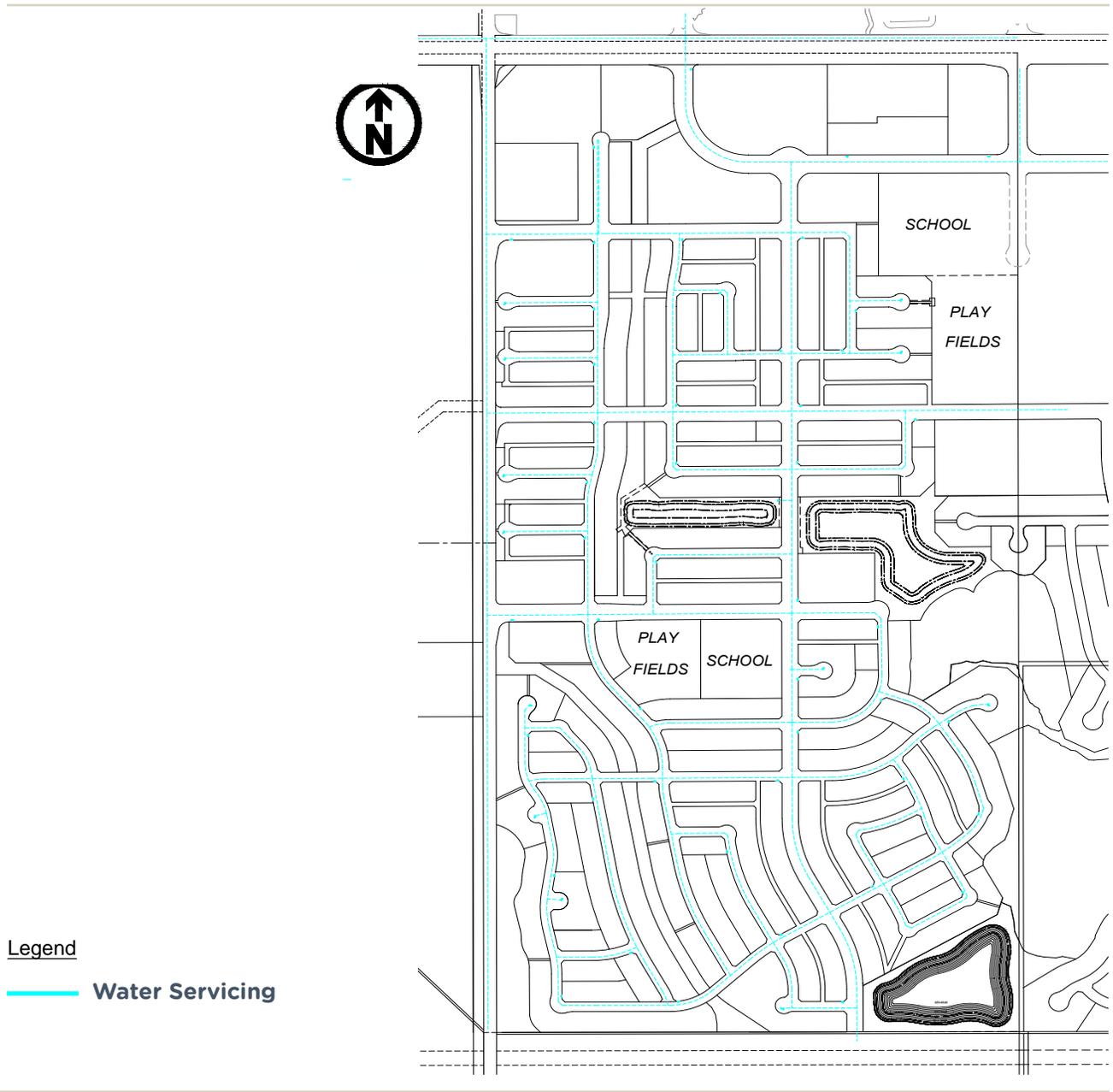
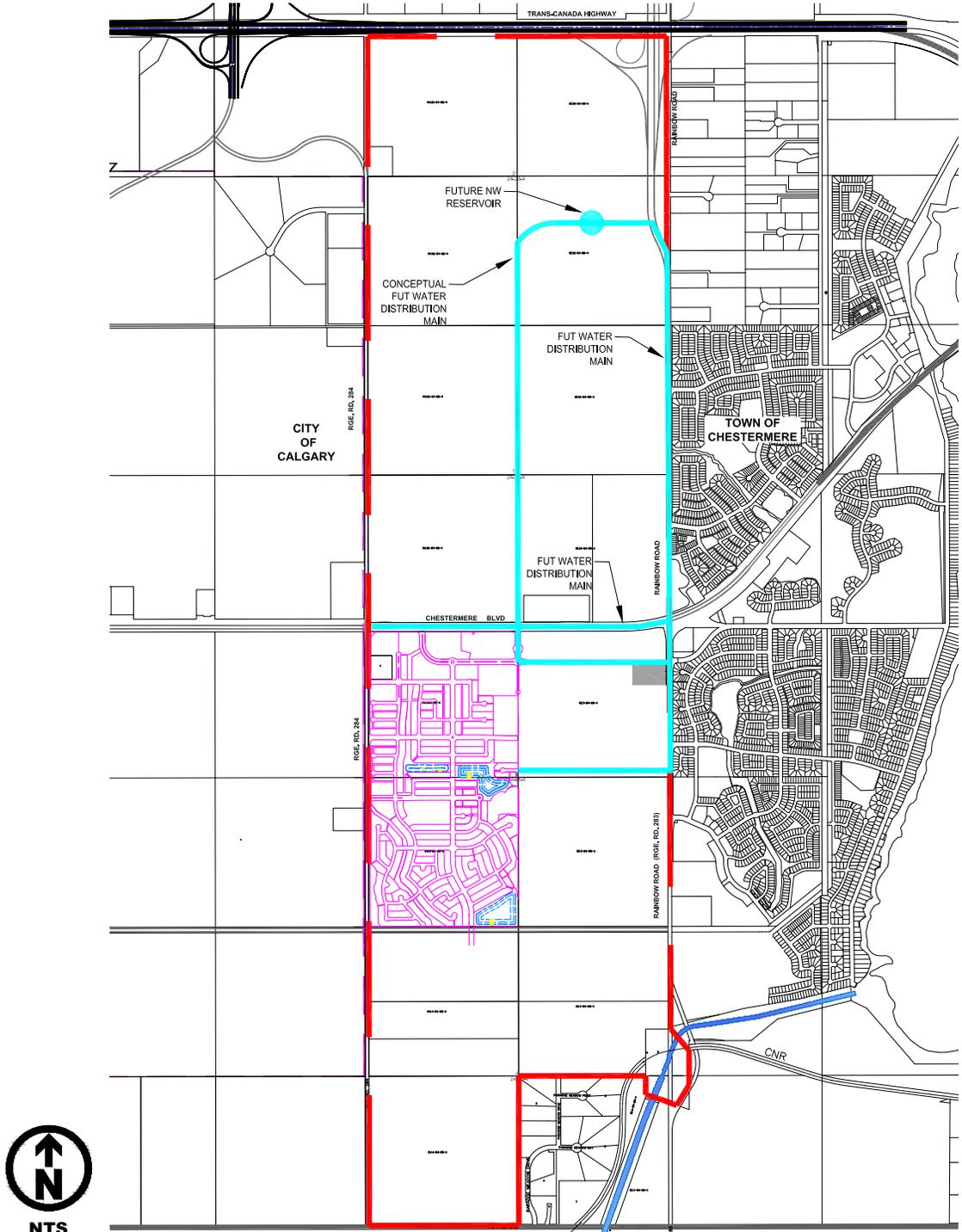


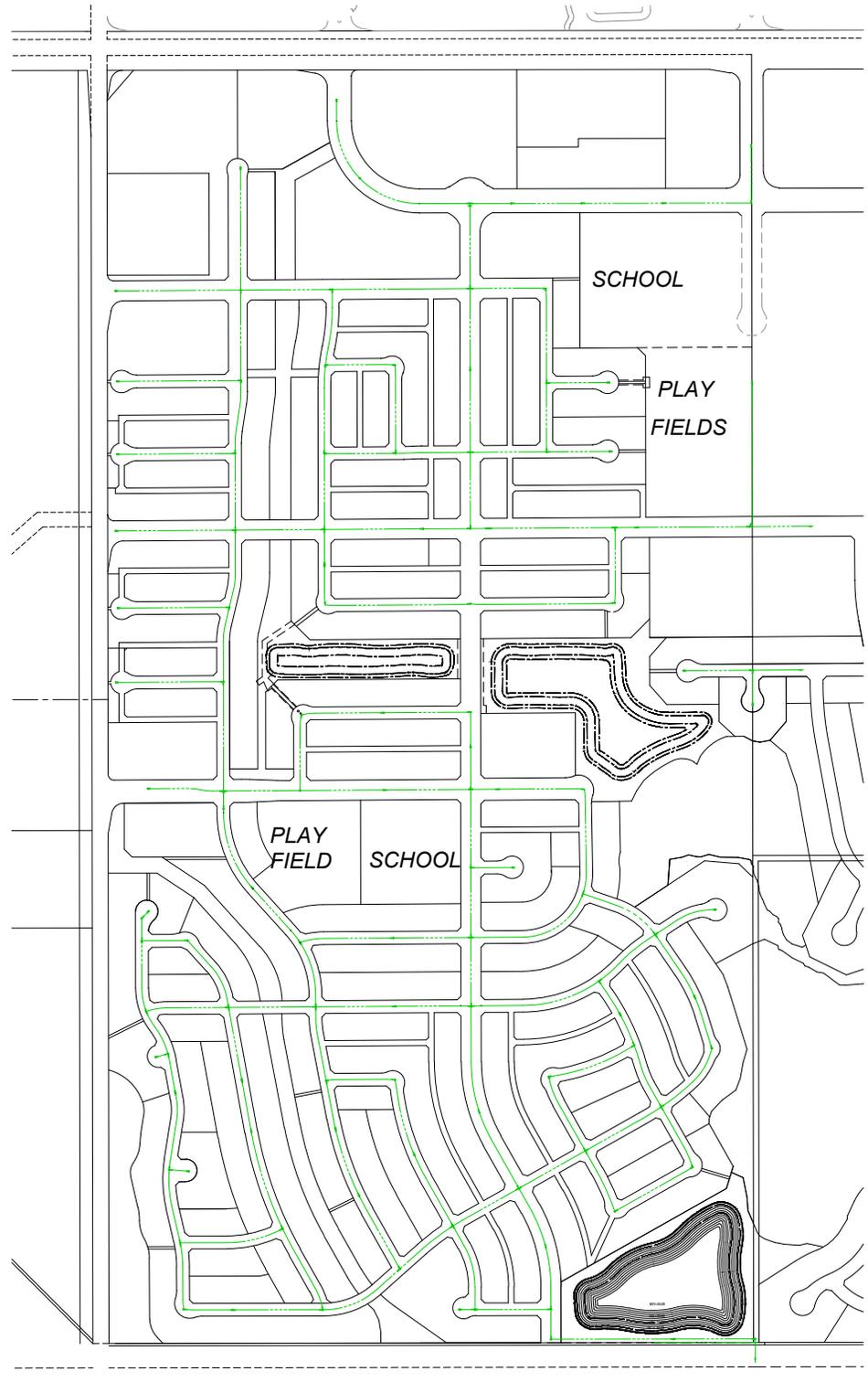
Figure 27b  
Offsite Water Connections



## 10.2 Sanitary Servicing

Wastewater from the Chelsea lands will be discharged into the proposed sanitary sewer trunk on Rainbow Road, which ultimately conveys the flow to Lift Station # 13, from which it will be pumped to the City of Calgary. Construction of Lift Station # 13 is underway with completion expected in 2016. The proposed sanitary trunk on Rainbow Road is part of the Offsite Levy Bylaw, with construction anticipated in 2016-2017. The Chelsea sanitary sewer collector pipes will tie to the collector pipes in the proposed adjacent development at the east boundary, which will ultimately connect to the proposed sanitary trunk on Rainbow Road.

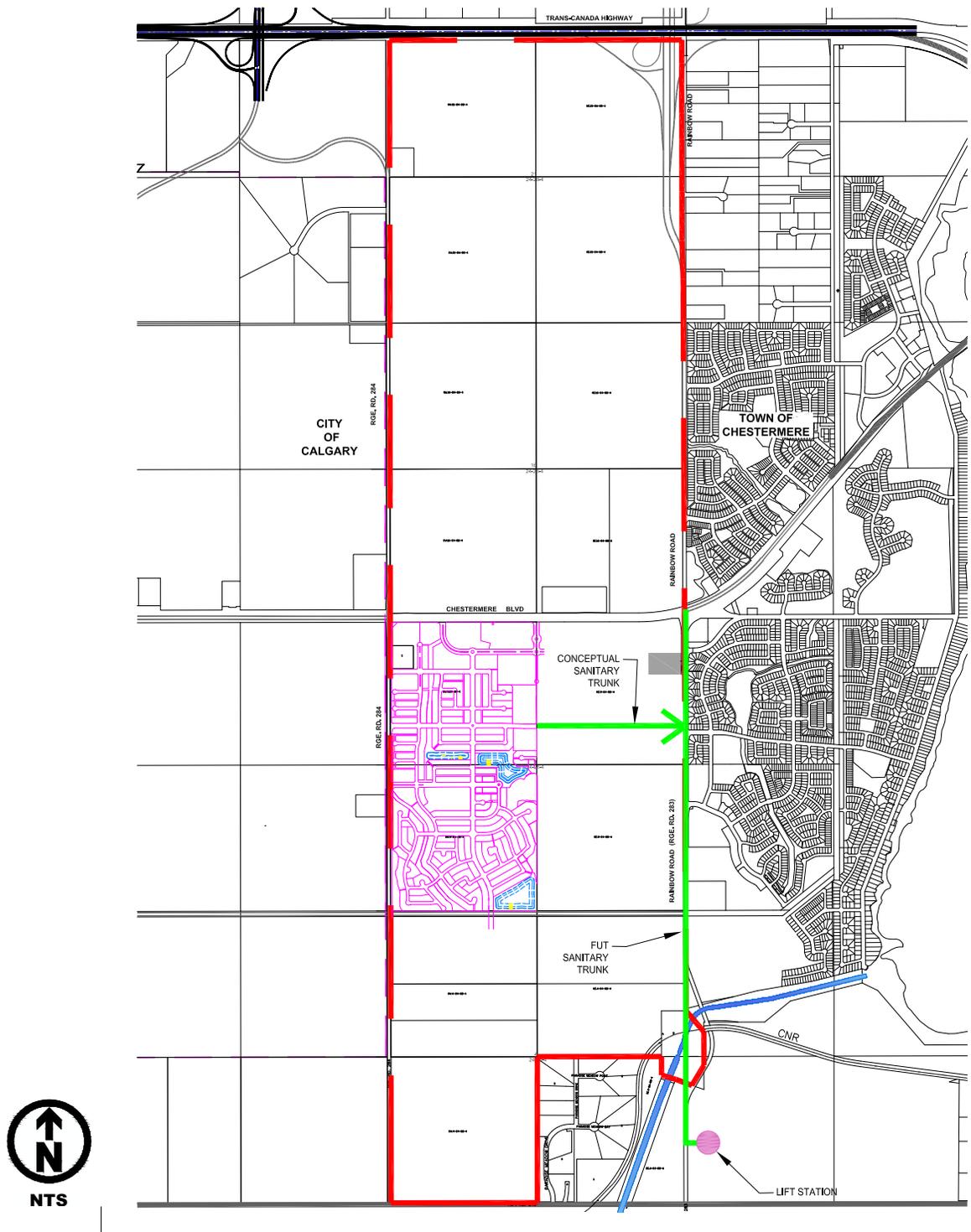
Figure 27c  
Sanitary Servicing



Legend

-  Sanitary Servicing
-  Stormwater Pond

Figure 27d  
Offsite Sanitary Connections



## 10.3 Stormwater Servicing

Stormwater Servicing for the entire Outline Plan area will be by gravity sewer system constructed within proposed roadways and right of ways. The stormwater runoff will be conveyed to three on-site stormwater ponds. Two ponds located centrally and one located in the southwest corner of the outline plan area. The stormwater pond will control the quantity of runoff and infrastructure will be in place to improve the quality of runoff prior to entering the pond. The stormwater management strategies achieve the regional stormwater targets outlined in Chestermere's Integrated Stormwater Master Plan (ISMP). The site controls discharge to 0.8 L/s/ha and 40mm of average annual runoff. The ultimate discharge location for stormwater will be determined by the City of Chestermere and Chestermere Utilities Incorporated and future Utility Master Plans. In the absence of a regional stormwater outlet the site manages stormwater on a zero discharge basis through the implementation of enhanced evaporation.

Best Management Practices and Low Impact Development strategies will be implemented to retain and use the stormwater on-site the irrigation of open space and the integration of wetlands within the plan. The use of stormwater for irrigation and maintaining wetland hydrology is a key component of the ultimate and interim stormwater management strategy.

Figure 27e  
Stormwater Servicing

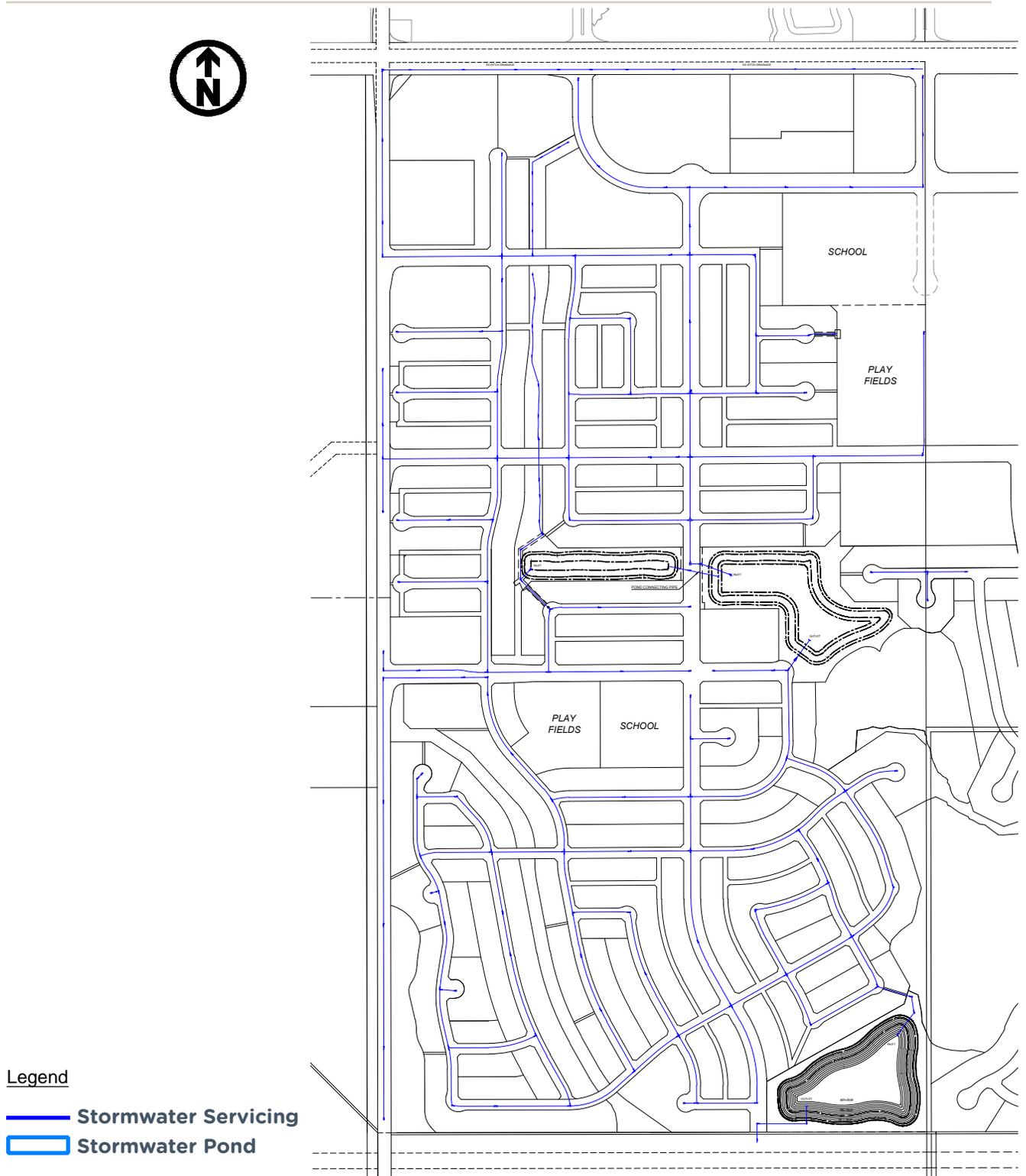
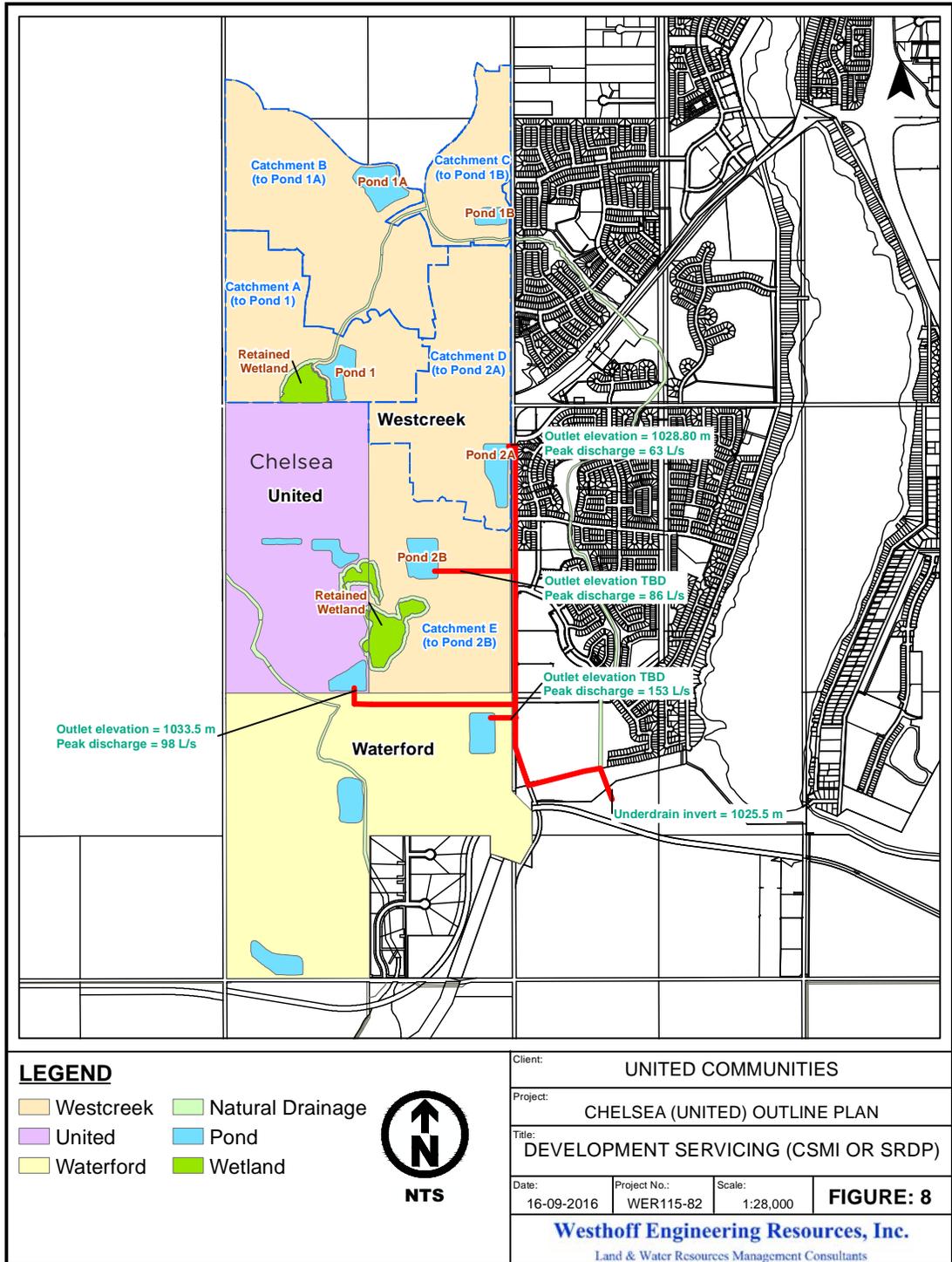


Figure 27f  
Offsite Stormwater Connections



## 10.4 Shallow Utilities

Gas, power, telephone, cable TV and other appropriate private services are to be provided to the site. The necessary easements and utility Right-of-Ways shall be provided to each of the proposed lots to the specifications of the utility company at the time of subdivision



## Section 11.0

# Supporting Studies



Several studies were completed to ensure that the outlined development was appropriate for the subject lands. A summary of the findings of these studies is shown below. All the documents have been submitted to the City under separate cover.

## 11.1 Geotechnical Evaluation

A geotechnical evaluation was completed on the subject property by McIntosh Lalani Engineering Ltd. in 2016 to evaluate subsurface soil and groundwater conditions on site and to provide geotechnical recommendations for the design and construction of the plan area.

The soil conditions encountered in test holes across the site generally consisted of topsoil overlaying silty clay till soils extending to sedimentary bedrock. The groundwater condition is mostly that of perched groundwater with a difference in water level elevations less than 1 metre, reflective of the regional groundwater table. The subsurface conditions were deemed suitable for foundation support for this development provided the recommendations within the report are followed. Portions of existing on-site materials may be suitable for use as general engineered fill subject to material evaluation and removal of deleterious materials. Imported fill should be approved for use as structural or general engineered fills.

## 11.2 Historical Resources Impact Assessment

Historic Resources Act clearance was granted for the subject area in March 2010. Stantec Consulting Ltd. prepared a Historic Resources overview assessment which indicated that the potential of the development to impact palaeontological or archaeological resources is low. Heritage Survey forms and associated documentation were submitted for a farmyard within the subject area that contains historic period structures, and clearance has been granted to proceed with development of the site including the area occupied by the historic period structures. Notwithstanding, where sites of archaeological or historical importance are identified during excavation, it is important that these sites are addressed prior to development.



## 11.3 Stormwater Master Drainage Plan

The Staged Master Drainage Plan included with the outline plan application addresses the management of stormwater under interim and ultimate conditions in accordance with the approved Master Drainage Plan.

## 11.4 Transportation Impact Assessment

A Transportation Impact Assessment (TIA) has been completed for the proposed outline plan, and is provided as part of this package. The TIA assumed future extension of Rainbow Road to Highway 1, as suggested in the City's Transportation Master Plan Update to occur with future developments within the area. Potential mitigation measures at the intersections of Highway 1 / Rainbow Road and Highway 1 / Conrich Road due to the re-routing of regional traffic with the Rainbow Road extension should be discussed between the City and Alberta Transportation (AT). The results of the TIA indicate the need for future signalization of the Chestermere Boulevard / Conrich Road intersection as a result of the full-buildout of both the subject lands and adjacent development. In addition, assuming appropriate signal warrants are met, future traffic signals are recommended with full build-out of the subject lands at several intersections (as noted in Figure 4.3 of the TIA). Figure 4.5 of the TIA indicates the proposed roadway classifications of the internal roadway network within the subject lands. It is anticipated that as future developments within the area continue to progress, Chestermere Boulevard will begin to transform into an urban corridor through the plan area, supporting the need for strong cross-street connectivity. The TIA also indicates strong pedestrian and cycling network connectivity both within the subject lands and to surrounding attractions and destinations.

## 11.5 Biophysical Impact Assessment

Westhoff Engineering Resources Inc. (Westhoff) prepared a Biophysical Impact Assessment (BIA) and Wetland Mitigation Strategy for the subject lands in 2016. The BIA describes existing environmental conditions and predicts the possible impacts of the proposed Project. Mitigation measures are recommended to reduce, eliminate or compensate for these potential impacts.

Existing environmental conditions are described for landforms, hydrology, soils, plant communities, wetlands and wildlife. Terrain consists of gently undulating plains with a higher ridge in the western portion of the property. An ephemeral drainage feature occurs in the southwest portion of the Project Site. Five plant communities are identified that include cultivated, disturbed, hayfield, aspen forest, drainage and wetland. A number of invasive plants were identified including three “Noxious” weeds: common toadflax, creeping thistle, perennial sow-thistle

Westhoff identified a total of 29 natural wetlands and one artificial wetland within the Project Site. The wetlands range in size from 0.03 to 3.3 ha for a total of 9.85ha and vary from Class I to Class V based on the Stewart and Kantrud Classification System. Wetlands S1, S2, S3/5 and 6 qualify as Aquatic Environmentally Significant Areas based on provincial criteria. One rare plant, yellow cress was identified at Wetlands 101, 109 and 111 however no rare ecological communities were observed.

A total of approximately 154 birds, 29 mammals, 3 reptiles, and 6 amphibians have the potential to occur based on species distributions. However, the majority of the subject site (80%) has been modified through cultivation. Although wildlife may travel and forage within the Project Site, in general, habitat conditions in the uplands are considered relatively poor with little or no natural upland vegetation communities remaining.

General mitigation measures are identified to reduce, eliminate, or control the potential negative impacts of the proposed subject development on existing environmental conditions. These include Erosion and Sediment Control measures, environmental construction BMPs, Landscape and weed management strategies, soil salvage for rare plants, sensitive timing of construction to avoid wildlife, and monitoring. The wetland mitigation strategy includes the retention of Wetlands S3/5, 6 and 112 and compensation for the loss of the remaining wetlands

No significant residual impacts on the environment are predicted to result from the proposed development. Equally, no cumulative effects are expected when the proposed development is considered in the context of past, current and future land uses in the region.

## 11.6 Growth Management

Section 13.3.2 (1) of the Chestermere Gateway ASP states that,

*“In conjunction with an Outline Plan and Land Use Amendment, a Growth Management Analysis shall be required to address the co-ordination of development, servicing and financing of infrastructure within the subject site.”*

Accordingly, B&A Planning Group prepared a Growth Management Analysis (June 2016) for the Outline Plan area to address these items, which has been provided as part of this package.

The report concluded that the proposed land use pattern, projected population and projected density for the subject lands are in alignment with the direction given in the Gateway ASP and Westbridge MASP. The subject lands are part of Community C within the Waterbridge MASP, which is identified as the first stage of development within Chestermere’s new growth area. In addition, development timing of the Chelsea Outline Plan area will address an immediate demand for new residential land supply in Chestermere.

Municipal utility and transportation infrastructure required to service the subject lands is covered by off-site levies under the 2016 Draft Chestermere Off-Site Levy Bylaw. The Off-Site Levy Bylaw indicates that in cases where the off-site levy reserve funds will not be sufficient to pay for the construction of infrastructure items at the time they are required, or where an infrastructure item is required in advance of the municipal projected timing, front ending of infrastructure may be required.



## Section 12.0

# Implementation



## 12.1 Phasing

Chelsea will be developed in 21 phases, as shown in Figure 28.

The phase boundaries have been ordered sequentially based on servicing reviews and forecasts for housing absorption. Development will generally proceed from north to south. Phasing in this manner allows servicing to be extended in a logical manner and connected to existing services.

Phasing will be coordinated to provide full and emergency access as required over the development of the neighbourhood.

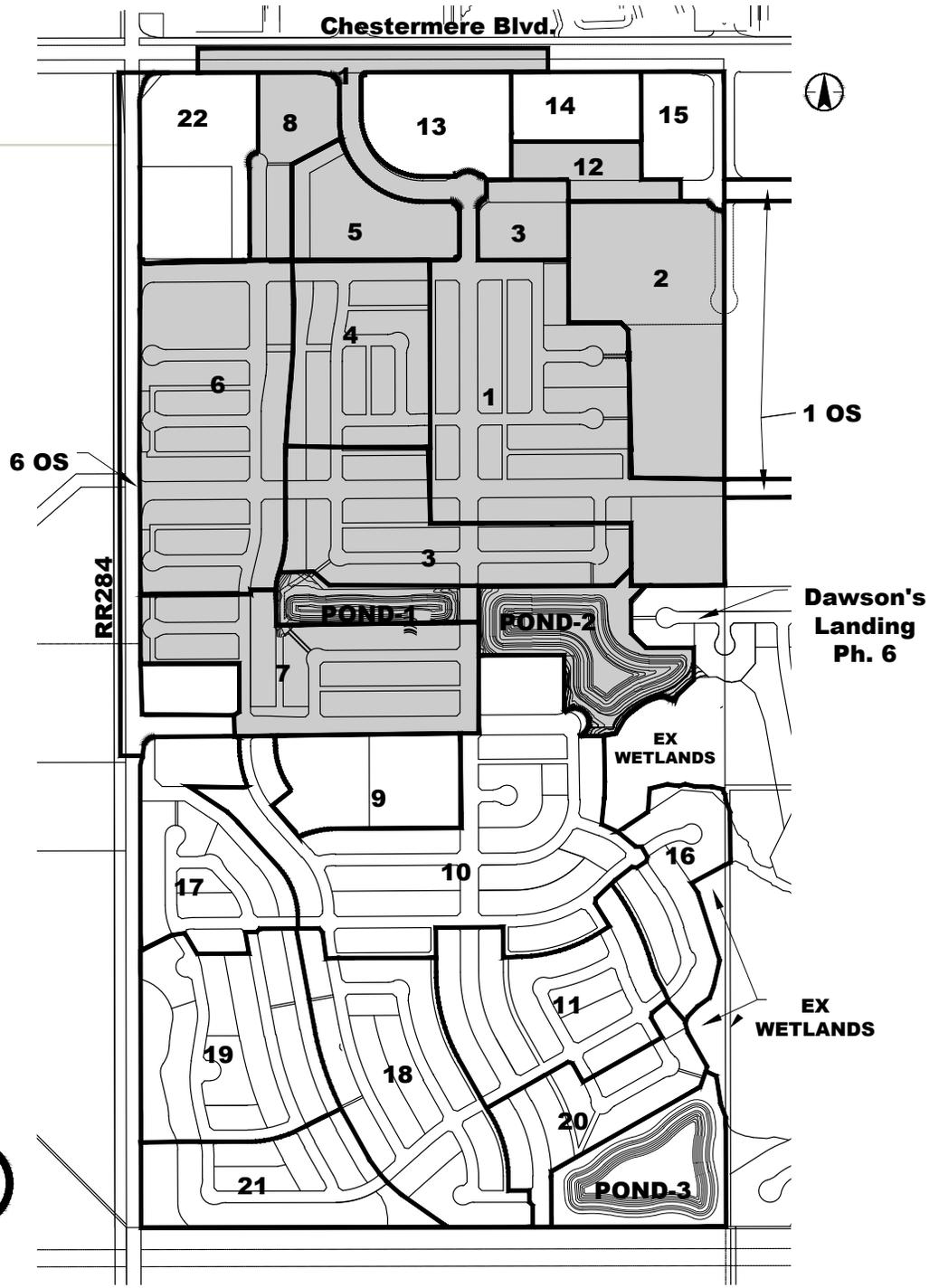
Actual phasing size and location will be dependent on future market conditions and infrastructure requirements.

## 12.2 Funding of Regional Services

Throughout a number of overarching planning documents and policies, a range of essential services and non-essential facilities are identified and planned for in order to support and service future growth and/or existing population. Due to the regional nature and city-wide benefit of many of these services and facilities a funding contribution strategy will be fairly and equitably negotiated between the City and the development industry and implemented at the subdivision stage.



Figure 28  
Phasing Plan





# Appendix

## Appendix A: Policy Conformance

## Chelsea Land Use & Outline Plan – Gateway ASP Policy Conformance

Applicable Policies	Conformance
<b>6.3 COMMUNITY IDENTITY</b>	
<b>(1) Community Names</b>	
In conjunction with the initial Outline Plan and Land Use Amendment submission for all or any portion of a community or the Town Centre, a community naming application should be submitted and approved by Council in order to define the community name for the subject area.	No formal community name application form is available. This land use and outline plan report proposes the subject site to be named <i>Chelsea</i> .
<b>(2) Local Street Names</b>	
In conjunction with the initial Outline Plan and Land Use Amendment application submitted within an ownership area in each Community and the Town Centre, a street name application shall be submitted for the approval of Council, identifying the proposed street names that are intended to be applied within the Outline Plan area.	No formal street name application form is available. Proposed street names are identified on the outline plan.
<b>(3) Arterial Street Names</b>	
In recognition of the intracommunity function of the westerly arterial/collector street that divides Community “A” and “B”, and the east/ west arterial street that divides Community “B” and “C,” respectively, these streets should be given a common name that is not directly related to the adjacent community name.	The east/west arterial is already named <i>Chestermere Boulevard</i> .
<b>(4) Compatible Interface</b>	
(a) The western edge of the development within the City, located along Conrich Road, shall provide for a compatible interface with this road in accordance with the City’s entranceway requirements.	<i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 6.10: Range Road 284 Interface</i> provides information on the proposed interface with Range Road 284. .
(b) At the Outline Plan Stage, plans and visual information should be provided to allow for the assessment of the interface with the plans to show: <ul style="list-style-type: none"> <li>• landscaping;</li> <li>• fencing;</li> <li>• lotting;</li> <li>• parking; and</li> <li>• building orientation</li> </ul>	<i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 8: Overall Concept Plan</i> presents the interface with Conrich road, including details regarding lotting, parking and building orientation. <i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 6.4: Transitional Use Corridor</i> provides details on Gateway Entrance Feature at Conrich Road/Chestermere Boulevard and <i>Section 6.10: Range Road 284 Interface</i> identifies the proposed fencing along Conrich Road.
<b>8.1 RESIDENTIAL AREAS</b>	
<b>8.1.2 (2) Density Range</b>	
(a) The minimum required residential density shall be in accordance with the previous of the Municipal Development Plan.	The majority of the subject site falls into the New General Urban policy area of the <i>Municipal Development Plan</i> and Section 3.2.4 requires a density range of 5 to 7 units per gross acre. The residential density of the subject site is 7.5 upa. While this is slightly higher than that identified in the <i>Municipal Development Plan</i> , it nears the density target set in the policies of the <i>Calgary Metropolitan Plan</i> which requires 8 to 10 upa as per Strategy 3.a.5.

Applicable Policies	Conformance
<b>(4) Density and Housing Mix Analysis</b>	
(a) At the Outline Plan and Land Use Amendment approval stage, a Density Analysis should be submitted that addresses the proposed density within the subject area.	<i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 8.2: Density</i> provides a density analysis for the subject site.
<p>Detailed Neighbourhood Area and Density Policies are contained in Sections 8.0 and 9.0 of the Waterbridge MASP and should be referred to at the Outline Plan and Land Use Amendment review stage:</p> <p><i>8.2.2 Policies</i></p> <p><i>(2) Density of Neighbourhood Node</i></p> <p><i>A Neighbourhood Node shall contain a minimum of 90 dwelling units on 2.0 hectares (5 acres) of net developable land, which equates to a minimum density of 45 units per net hectare (18 units per net acre).</i></p>	<p>In compliance with this policy, the proposed neighbourhood node contains approximately 133 units on 2.9 hectares (7.1 acres) of net developable land, which equates to a minimum density of 46.5 units per net hectare (18.8 units per net acre).</p>
7.2 Neighbourhood Nodes	
<b>7.2.2 Policies</b>	
<b>(1) Composition of Neighbourhood Nodes</b>	
<p>(a) As required in the Waterbridge MASP, the Neighbourhood Nodes should consist of,</p> <ul style="list-style-type: none"> <li>(i) multi-residential uses,</li> <li>(ii) a central park or school site,</li> <li>(iii) an interconnected road or pedestrian system, and</li> <li>(iv) transit stops for future bus transit</li> </ul> <p>(b) Where appropriate a Neighbourhood Node may also contain local commercial, recreational, institutional or employment-related uses.</p>	<p>The Chelsea Neighbourhood Node complies with the requirements of the ASP:</p> <ul style="list-style-type: none"> <li>• Multi-residential uses in the form of apartments and townhouses are provided.</li> <li>• The Neighbourhood Node includes a school site.</li> <li>• The Neighbourhood Node is located along two collector roads: Chelsea Drive and Chelsea Park Walk. Sidewalks run along these collectors and the Node is connected to the Regional Pathway.</li> <li>• Future bus stops are identified at the centre and western portion of the node.</li> <li>• The Neighbourhood Node will contain commercial uses in the form of stand alone or mixed use buildings.</li> </ul>
<b>(3) Design of Neighbourhood Nodes</b>	
The Chestermere Design Guidelines shall be referenced in the design review of multi-family residential development within a Neighbourhood Node.	<i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 8: Overall Concept Plan</i> provides an impression of how the multi-residential developments will appear. A demonstration of compliance with the <i>Chestermere Design Guidelines</i> will be provided at the development permit stage as specific design details will be provided at that time.

Applicable Policies	Conformance
<p><b>(4) Analysis of Neighbourhood Nodes</b></p> <p>(a) Outline Plan and Land Use Amendment application shall include</p> <p>(i) a density analysis for the Neighbourhood Node, and</p> <p>(ii) a description of the composition and design of the Neighbourhood Node.</p>	<p>In compliance with this policy, the proposed neighbourhood node contains approximately 133 units on 2.9 hectares (7.1 acres) of net developable land, which equates to a minimum density of 46.5 units per net hectare (18.8 units per net acre).</p> <p>The composition of the Neighbourhood Node is as follows:</p> <ul style="list-style-type: none"> <li>• Multi-residential uses in the form of apartments and townhouses are provided.</li> <li>• The Neighbourhood Node includes a school site;</li> <li>• The Neighbourhood Node is located along two collector roads: Chelsea Drive and Chelsea Park Walk. Sidewalks run along these collectors and the Node is connected to the Regional Pathway.</li> <li>• Future bus stops are identified at the centre and western portion of the node.</li> <li>• The Neighbourhood Node will contain commercial uses in the form of stand alone or mixed use buildings.</li> </ul>
<p>(b) At the outline plan stage a development concept plan for a Neighbourhood Node shall also be provided showing its potential development pattern including buildings, streets, pathways and open space.</p>	<p><i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 8: Overall Concept Plan</i> provides an impression of how the multi-residential development will appear.</p>
<p>Detailed Neighbourhood Node Policies are contained in Section 8.2 of the Waterbridge MASP and should be referred to at the Outline Plan and Land Use Amendment review stage:</p> <p><i>8.2.2 Policies</i></p> <p><i>(1) Composition of Neighbourhood Node</i></p> <p><i>(a) Subject to the policies of this Master ASP,</i></p> <p><i>i. the predominant use of land in a Neighbourhood Node shall include</i></p> <p><i>A. a variety of street-oriented multi-dwelling Residential uses such as semi-detached dwellings, townhouses and apartments,</i></p> <p><i>B. locations for future transit bus stops,</i></p> <p><i>C. a mixed use development project subject to</i></p>	<p>The composition of the Neighbourhood Node is as follows:</p> <ul style="list-style-type: none"> <li>• Multi-residential uses in the form of apartments, and townhouses are provided.</li> <li>• <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 22: Transit Network</i> identifies the location of bus stops in the node.</li> <li>• The Neighbourhood Node includes a mix of uses including commercial, residential and open space.</li> <li>• A school site is provided within the node that is 3.24 ha (8.00 ac) in size and has the potential to be orientated towards the</li> </ul>

Applicable Policies	Conformance
<p><i>Policy (3), and</i></p> <p><i>D. a park or central amenity space, subject to subsection (b) below;</i></p> <p><i>ii. the following uses may be allowed in a Neighbourhood Node where determined to be compatible and appropriate:</i></p> <p><i>A. a limited number of single-detached dwellings;</i></p> <p><i>B. local commercial uses, recreational uses and institutional uses,</i></p> <p><i>C. civic uses,</i></p> <p><i>D. an elementary or middle school site, provided the school building will be appropriately oriented toward the node;</i></p> <p><i>iii. similar uses, public uses and accessory uses may also be allowed throughout a Neighbourhood Node where determined to be compatible and appropriate.</i></p> <p><i>(b) Where an elementary or middle school site is located within a Neighbourhood Node, a park or central amenity space is not required.</i></p> <p><i>(c) The general category of uses identified under subsection</i></p> <p><i>(a) shall be refined through the Outline Plan and the Land Use Districts applied to the Neighbourhood Node.</i></p> <p><i>(2) Density of Neighbourhood Node</i></p> <p><i>A Neighbourhood Node shall contain a minimum of 90 dwelling units on 2.0 hectares (5 acres) of net developable land, which equates to a minimum density of 45 units per net hectare (18 units per net acre).</i></p> <p><i>(3) Mixed Uses in Neighbourhood Node</i></p> <p><i>(a) Consideration shall be provided in the Neighbourhood Node for non-residential development such as local commercial uses, school buildings, civic uses (cultural, recreational or institutional facilities) or employment uses, in either a mixed use building or as a stand-alone development on a site.</i></p> <p><i>(b) Non-residential development in a Neighbourhood Node shall</i></p> <p><i>i. be oriented to the street and have direct pedestrian connections from the sidewalk to building entrances,</i></p> <p><i>ii. be small-scale and compatible with adjacent residential uses, and</i></p> <p><i>iii. not be automotive-based or involve drive-up services.</i></p>	<p>node.</p> <ul style="list-style-type: none"> <li>• In compliance with this policy, the proposed neighbourhood node contains approximately 94 units on 0.96 hectares (2.36 acres) of net developable land, which equates to a minimum density of 97.9 units per net hectare (39.8 units per net acre).</li> <li>• The Neighbourhood Node is located along two collector roads: Chelsea Drive and Chelsea Boulevard. Sidewalks run along these collectors and the Node is connected to the Regional Pathway.</li> <li>• Future bus stops are identified at the centre and western portion of the node.</li> <li>• The layout of the node includes tree lined streets, lanes and rear drive access.</li> <li>• No cul-de-sacs or p-loops are located within the node.</li> <li>• Sustainable design initiatives will be considered at the development permit stage.</li> </ul>

Applicable Policies	Conformance
<p><i>(4) Amenity Space in Neighbourhood Node</i>  <i>The Amenity Space in a Neighbourhood Node should be</i></p> <ul style="list-style-type: none"> <li><i>i. comprised of a plaza, green or square, or other formal park space,</i></li> <li><i>ii. between 0.2 hectares and 1.0 hectares (.5 acres and 2.5 acres) in size,</i></li> <li><i>iii. bounded by streets or building facades, and</i></li> <li><i>iv. located in close proximity to one or more transit stops.</i></li> </ul> <p><i>(5) Transit Service to Neighbourhood Node</i>  <i>A Neighbourhood Node shall be capable of accommodating public transit and contain a defined area for</i></p> <ul style="list-style-type: none"> <li><i>i. bus transit stop(s), and</i></li> <li><i>ii. related transit-service facilities such as lighted shelters with seating for patrons and secure bicycle parking.</i></li> </ul> <p><i>(6) Streetscape for Neighbourhood Node</i>  <i>(a) A Neighbourhood Node should contain</i></p> <ul style="list-style-type: none"> <li><i>i. a block-based design,</i></li> <li><i>ii. lanes and rear drive access,</i></li> <li><i>iii. tree-lined streets, and</i></li> <li><i>iv. street-oriented buildings with minimal and uniform front-yard setbacks.</i></li> </ul> <p><i>(b) Except when unavoidable due to site constraints, the use of cul-de-sacs and p-loops, and similar self-contained cells, should be avoided within a Neighbourhood Node in favour of an interconnected road network consisting of multiple and parallel routes for pedestrians and vehicles along the public street.</i></p> <p><i>(7) Sustainable Design of Neighbourhood Nodes</i>  <i>Neighbourhood Nodes should include sustainable development and design initiatives such as:</i></p> <ul style="list-style-type: none"> <li><i>i. green building and design best practices</i></li> <li><i>ii. green technology including water and energy saving initiatives</i></li> <li><i>iii. universal and barrierfree design features.</i></li> </ul>	
<b>9.0 Town Centre</b>	
<b>9.1.2 Policies</b>	
<b>(1) Composition of General Commercial Area</b>	
<p>The General Commercial Area,</p> <ul style="list-style-type: none"> <li>(i) shall contain predominantly medium to larger format commercial uses, with the size and form of</li> </ul>	<p>The proposed General Commercial Area is proposed to be redesignated to the Local Commercial (C) District which accommodates a</p>

<b>Applicable Policies</b>	<b>Conformance</b>
<p>these uses to be subject to the policies of the MDP, (ii) may contain a variety of smaller format commercial uses, as well as office, institutional, recreational, public and similar uses that are consistent with and support the function of the area.</p>	<p>variety of commercial uses.</p>
<b>(2) Design of General Commercial Area</b>	
<p>The Land Use Districts applied to the General Commercial Area should contain standards that address the site and building design in this area.</p>	<p>The proposed General Commercial Area is proposed to be redesignated to the Local Commercial (C) District which provides standards for site and building design.</p>
<b>(3) Block Layout of General Commercial Area</b>	
<p>(a) The General Commercial area layout shall be characterized by blocks defined by an interconnecting and parallel street network. (b) The Outline Plan for the General Commercial Area should determine the layout of the General Commercial Area.</p>	<p>The General Commercial Area is defined as one large block by Chestermere Boulevard to the north, Chelsea Boulevard to the south, the Transitional Use Area to the west and Chelsea Entrance to the east.</p> <p>A conceptual layout of the General Commercial Area is shown in the <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 10: General Commercial Area and Transitional Use Corridor Concept Plan</i> that provides an impression of how the commercial development will appear. The final layout will be determined at the development permit stage.</p>
<b>(4) Analysis for General Commercial Area</b>	
<p>(a) In conjunction with the Outline Plan the following analysis should be provided: (i) a Concept Development Plan showing the buildings, parking, landscaping and other features of the area, and (ii) a set of design guidelines addressing the density and form of development on the site for incorporation into the Land Use Districts to be applied to the area. (b) Appropriate market rationale that supports the anticipated size of the commercial units to be developed in the area may also be required to be submitted.</p>	<p>For the General Commercial Area, submission of:</p> <ul style="list-style-type: none"> <li>• A conceptual layout of the General Commercial Area is shown in the <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 10: General Commercial Area and Transitional Use Corridor Concept Plan</i> that provides an impression of how the commercial development will appear. The final layout will be determined at the development permit stage.</li> <li>• For design guidelines see <i>Chelsea Outline Plan &amp; Land Use Redesignation, Appendix B: Chelsea Commercial Design Guidelines</i>.</li> <li>• Market rationale will be provided at the development permit stage if required by the City.</li> </ul>
<p>Detailed Town Centre Policies are contained in Section 8.5 of the Waterbridge MASP and should be referred to at the Outline Plan and Land Use Amendment review stage:</p>	<p>The proposed General Commercial Area is proposed to be redesignated to the Local Commercial (C) District which accommodates large, medium and small commercial uses as well as office and public uses.</p>

Applicable Policies	Conformance
<p><b>8.5.2 Policies</b></p> <p><i>(1) Composition of the Town Centre</i></p> <p><i>(b) Subject to the policies of this Master ASP,</i></p> <p><i>i. the predominant use of land in the General Commercial Area shall be medium to larger format retail commercial uses that serve as anchor tenants for the Town Centre, in accordance with the MDP.</i></p> <p><i>ii. in addition to (i.), the following uses may also be allowed in the General Commercial Area where determined to be compatible and appropriate:</i></p> <p><i>A. office and other employment related uses;</i></p> <p><i>B. civic uses, institutional uses and recreational uses;</i></p> <p><i>C. meeting and conference facilities;</i></p> <p><i>D. public uses, including a BRT station;</i></p> <p><i>E. wetlands, sports fields, walking trails;</i></p> <p><i>F. multi-dwelling residential uses;</i></p> <p><i>G. accessory uses and public uses;</i></p> <p><i>H. service commercial uses, including hotels.</i></p>	
<b>9.3.2 Policies</b>	
<b>(1) Composition of Transitional Use Corridor</b>	
<p>(a) In accordance with the policies in the MASP, the Transitional Use Area should include a variety of low to medium density residential uses, together with other compatible and appropriate uses.</p> <p>(b) Vertical and horizontal mixing of uses should be accommodated within the Transitional Use Corridor.</p>	<p>A variety of low to medium density residential uses are proposed for the Transitional Use Corridor as defined by the proposed land use districts.</p>
<b>(2) Orientation of Transitional Use Corridor</b>	
<p>Residential and commercial buildings should be oriented toward the front street with parking located to the rear or side of the building.</p>	<p>A conceptual layout of the Transitional Use Corridor is shown in the <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 10: General Commercial Area and Transitional Use Corridor Concept Plan</i> and provides an impression of how the commercial development will appear. As shown in Figure 10, residential and commercial buildings are envisioned to frame the streets with parking located internally within each parcel. The final layout will be determined at the development permit stage.</p>
<b>(3) Design in Transitional Use Corridor</b>	
<p>Multi-family residential development should be subject to the <i>Chestermere Design Guidelines</i> and/or other municipal policies.</p>	<p>A demonstration of compliance with the <i>Chestermere Design Guidelines</i> will be provided at the development permit stage as specific design</p>

Applicable Policies	Conformance
<p>Detailed Transitional (Mixed Use) Corridor Policies are contained in Section 8.4 of the Waterbridge MASP and should be referred to at the Outline Plan and Land Use Amendment review stage:</p> <p><i>(a) Subject to the policies in this Master ASP,</i></p> <p><i>i. the predominant use of land within the Mixed Use Corridor shall be multi-unit dwellings;</i></p> <p><i>ii. the following uses may be allowed within the Mixed Use Corridor where determined to be compatible and appropriate:</i></p> <p><i>A. local commercial uses, service commercial uses and secondary commercial uses;</i></p> <p><i>B. civic uses, institutional uses and recreational uses;</i></p> <p><i>C. secondary suites and other innovative housing forms; and</i></p> <p><i>D. single-detached residential uses in limited and defined areas.</i></p> <p><i>iii. similar uses, public uses and accessory uses may also be allowed within the Mixed Use Corridor where determined to be compatible and appropriate.</i></p> <p><i>(b) Commercial development within the Mixed Use Corridor shall complement the function of the corridor and support its long-term viability.</i></p> <p><i>(c) The general categories of uses identified under subsection (a) above shall be refined through the Outline Plan and Land Use Districts applied to the Mixed Use Corridor.</i></p> <p><i>(2) Development Integration in Mixed Use Corridor Uses may be vertically or horizontally integrated within a building or on a site within the Mixed Use Corridor.</i></p> <p><i>(3) Residential Orientation in Mixed Use Corridor Residential development shall not be required to be oriented to Chestermere Boulevard in the Mixed Use Corridor.</i></p> <p><i>(4) Sustainable Design of the Mixed Use Corridor The Mixed Use Corridor should include sustainable development and design initiatives such as:</i></p> <p><i>i. green building and design best practices;</i></p> <p><i>ii. green technology such as water and energy saving initiatives;</i></p> <p><i>iii. universal and barrier-free design features;</i></p> <p><i>iv. bicycle and pedestrian connectivity.</i></p> <p><i>(5) Design Guidelines for the Mixed Use Corridor Design of the Mixed Use Corridor will be subject to</i></p>	<p>details will be provided at that time.</p> <p>A variety of low to medium density residential uses are proposed for the Transitional Use Corridor as defined by the proposed land use districts. Other uses include municipal reserve to accommodate a gateway entrance feature for Chestermere and a park space and local pathway.</p> <p>It is envisioned that the residential buildings will frame Chestermere Boulevard and provide a sense of arrival to the City.</p> <p>Sustainable design initiatives and the Chestermere Design Guidelines will be considered at the development permit stage.</p>

Applicable Policies	Conformance
<i>site design and landscape requirements as set out in the Chestermere Design Guidelines.</i>	
<b>10.1 HIGH SCHOOL / REGIONAL RECREATIONAL CENTRE SITE</b>	
<b>10.1.2 Policies</b>	
<b>(1) Composition of High School / Recreational Centre</b>	
The high school and recreational centre site should be located and planned on an integrated basis at the south end of the Town Centre.	The outline plan identifies land to accommodate the future High School next to the future recreation site that will be located on the adjacent property to the east.
<b>(2) Analysis</b>	
<p>In conjunction with an Outline Plan and Land Use Amendment application the following analysis should be submitted:</p> <p>(i) a concept plan showing the potential development of the site, and demonstrating that its size and configuration is appropriate;</p> <p>(ii) a reserve analysis identifying the land purchase areas and reserve dedication areas for the sites.</p>	<p>For the High School site, this submission includes:</p> <ul style="list-style-type: none"> <li>• <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 12: High School Concept</i> presents a concept plan showing the potential development of the site, size and configuration; and</li> <li>• <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 19: Outline Plan and Section 8.3: Municipal Reserve Analysis</i> identifies the land purchase and reserve dedication areas.</li> </ul>
<p>Detailed Recreation Centre and School Site Policies are contained in Sections 8.7 and 8.8 of the Waterbridge MASP and should be referred to at the Outline Plan and Land Use Amendment review stage:</p> <p><b>8.7.2 Policies</b></p> <p><b>(1) Composition of School Sites</b></p> <p><i>Subject to the policies of this Master ASP,</i></p> <p><i>i. middle and elementary school sites shall accommodate the school and related playing fields, and</i></p> <p><i>ii. high school sites shall accommodate the high school, together with major playing fields and facilities.</i></p> <p><b>(2) Development of School Sites</b></p> <p><i>The following table will apply to the size, function and jurisdiction of the subject sites shown on the Land Use Concept Map:</i></p> <p><i>Table 8.7 Waterbridge School Sites</i></p> <p><i>Rocky View School Division</i></p> <p><i>High School 10.0 hectares (25.0 acres)</i></p>	<p>With regard to the school sites:</p> <ul style="list-style-type: none"> <li>• The subject outline plan accommodates a High School and two school sites together with associated playfields. One school site will include a school building and playfields while the other school site will only include playfields; the school building will be located on the adjacent property to the east.</li> <li>• The schools sites have been provided as per the current needs of the two schools boards.</li> <li>• Concept plans showing potential site designs for each school site are shown in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 12, 13 and 14</i>. More specific detail will be provided at the development permit stage by the applicable school board.</li> <li>• A shared private access is provided between the high school and recreation site to accommodate the integration of the two public uses.</li> </ul>

Applicable Policies	Conformance
<p><i>(3) Design of School Sites</i></p> <p><i>With the Outline Plan and Land Use Amendment application for an area containing a school site, a concept plan showing the site design on a school site should be prepared by the applicant, and accepted by the school board and/or the Town, that includes:</i></p> <ul style="list-style-type: none"> <li><i>i. building locations;</i></li> <li><i>ii. recreational facilities;</i></li> <li><i>iii. access and drop-off areas;</i></li> <li><i>iv. bus loading and unloading areas;</i></li> <li><i>v. parking areas;</i></li> <li><i>vi. school building;</i></li> <li><i>vii. preliminary site grading;</i></li> <li><i>viii. other information as determined necessary.</i></li> </ul> <p><i>(4) Integration of High School</i></p> <p><i>The high school should be integrated with the Town Centre, and the Recreation Centre, in terms of its location, orientation and design.</i></p>	
<b>10.2 PARKS</b>	
<b>10.2.2 Policies</b>	
<p><b>(1) Composition of Park Space</b></p> <p>A wide variety of park space shall be provided to meet a cross-section of user demands for passive and active recreational pursuits.</p>	<p>A variety of park space is provided within which includes linear parks, school play fields and an ecopark as identified in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 6.3: Open Space.</i></p>
<p><b>(2) Design of Park Space</b></p> <p>The location, size and function of a purposed park shall be determined at the Outline Plan and Land Use Amendment stage.</p>	<p>The location, size and function of each park is identified in in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 6.3: Open Space.</i></p>
<p><b>(3) Analysis for Park Space</b></p> <p>In conjunction with an Outline Plan and Land Use Amendment application the following analyses should be submitted:</p> <ul style="list-style-type: none"> <li>(i) a landscape concept plan for the park space;</li> <li>(ii) a reserve analysis.</li> </ul>	<p>Landscape concept plans for the park space and a reserve analysis are provided in in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 6.3: Open Space</i> and <i>Section 8.3: Municipal Reserve Analysis.</i></p>
<b>10.3 SCHOOL SITES</b>	
<b>10.3.2 Policies</b>	
<b>(1) Composition of School Sites</b>	
<p>A school site should consist of a school building envelope and sports fields, together with the related parking, access/ egress, student drop-off and landscaping components.</p>	<p>Concept plans showing potential site designs for each school site are shown in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 12, 13 and 14.</i> More specific detail will be provided at the</p>

Applicable Policies	Conformance
	development permit stage by the applicable school board.
<b>(2) Design of School Sites</b>	
School sites should be designed to achieve their educational and recreational function in the context of the surrounding neighbourhood.	Concept plans showing potential site designs for each school site are shown in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 12, 13 and 14</i> . More specific detail will be provided at the development permit stage by the applicable school board.
<b>(3) Analysis for School Sites</b>	
In conjunction with an Outline Plan and Land Use Amendment application a concept plan showing the layout of the school building envelope and sports field layout should be submitted	Concept plans showing potential site designs for each school site are shown in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 12, 13 and 14</i> . More specific detail will be provided at the development permit stage by the applicable school board.
<b>(4) Reserve Analysis</b>	
In conjunction with an Outline Plan and Land Use Amendment Application, a Reserve Analysis shall be submitted.	<i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 8.3</i> provides a reserve analysis.
<p>Detailed School Site Policies are contained in Sections 8.7 of the Waterbridge MASP and should be referred to at the Outline Plan and Land Use Amendment review stage.</p> <p><i>8.7.2 Policies</i></p> <p><i>(1) Composition of School Sites</i></p> <p><i>Subject to the policies of this Master ASP,</i></p> <p><i>i. middle and elementary school sites shall accommodate the school and related playing fields, and</i></p> <p><i>ii. high school sites shall accommodate the high school, together with major playing fields and facilities.</i></p> <p><i>(2) Development of School Sites</i></p> <p><i>The following table will apply to the size, function and jurisdiction of the subject sites shown on the Land Use Concept Map:</i></p> <p><i>Table 8.7 Waterbridge School Sites</i></p> <p><i>Rocky View School Division</i></p> <p><i>Elementary School 3.4 hectares (8.5 acres)</i></p> <p><i>Middle School 4.2 hectares (10.5 acres)</i></p> <p><i>Calgary Catholic School Board</i></p> <p><i>Elementary School 3.4 hectares (8.5 acres)</i></p> <p><i>Middle School 4.2 hectares (10.5 acres)</i></p>	<p>With regard to the school sites:</p> <ul style="list-style-type: none"> <li>• The subject outline plan accommodates a High School and two school sites together with associated playfields. One school site will include a school building and playfields while the other school site will only include playfields; the school building will be located on the adjacent property to the east.</li> <li>• The schools sites have been provided as per the current needs of the two schools boards based on our conversations with them.</li> <li>• Concept plans showing potential site designs for each school site are shown in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 12, 13 and 14</i>. More specific detail will be provided at the development permit stage by the applicable school board.</li> <li>• A shared private access is provided between the high school and recreation site to accommodate the integration of the two public uses.</li> </ul>

Applicable Policies	Conformance
<p><b>(3) Design of School Sites</b></p> <p><i>With the Outline Plan and Land Use Amendment application for an area containing a school site, a concept plan showing the site design on a school site should be prepared by the applicant, and accepted by the school board and/or the Town, that includes:</i></p> <ul style="list-style-type: none"> <li><i>i. building locations;</i></li> <li><i>ii. recreational facilities;</i></li> <li><i>iii. access and drop-off areas;</i></li> <li><i>iv. bus loading and unloading areas;</i></li> <li><i>v. parking areas;</i></li> <li><i>vi. school building;</i></li> <li><i>vii. preliminary site grading;</i></li> <li><i>viii. other information as determined necessary.</i></li> </ul> <p><b>(4) Integration of High School</b></p> <p><i>The high school should be integrated with the Town Centre, and the Recreation Centre, in terms of its location, orientation and design.</i></p>	
<b>10.5 WETLANDS</b>	
<b>10.5.2 Policies</b>	
<b>(1) Preservation of Major Wetlands</b>	
<p>The major wetland complexes within the ASP area that are identified for preservation should be:</p> <ul style="list-style-type: none"> <li>(a) acquired by the City through dedication as environmental reserve;</li> <li>(b) sustained through integration with the stormwater management system;</li> <li>(c) enhanced with recreational amenities and pathway amenities that complement and support its function;</li> <li>(d) integrated with the adjacent urban development in an appropriate manner.</li> </ul>	<p>The Major Wetland within the subject site will be dedicated as Environmental Reserve. It will be sustained through the provision of stormwater irrigation. Proposed recreational amenities include a viewing platform, group picnic area, playground, boardwalk and regional pathway. The Major wetland is integrated with adjacent urban development by acting as the terminus of the Neighbourhood Node (See <i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 6.8: Eco Park</i>).</p>
<b>(2) Analysis for Major Wetlands</b>	
<p>The following should be submitted in conjunction with an Outline Plan and Land Use Amendment application:</p> <ul style="list-style-type: none"> <li>(a) a biophysical impact assessment that identifies the specific type and extent of natural habitat features that should be retained as Environmental Reserve.</li> <li>(b) a staged master drainage plan identifying the method of integrating the hydrology of the major wetland with the overall stormwater management</li> </ul>	<p>The following is provided in response to this policy:</p> <ul style="list-style-type: none"> <li>a. A Biophysical Impact Assessment has been submitted under separate cover to the City.</li> <li>b. A Staged Master Drainage Plan has been submitted to the City under separate cover.</li> <li>c. A landscape concept presenting the proposed elements for the Major Wetland</li> </ul>

Applicable Policies	Conformance
<p>system.</p> <p>(c) a landscaping concept that illustrates how such elements as pathway connections, viewpoints, interpretive signage and related structures may be sensitively integrated with the major wetland's natural habitat areas.</p>	<p>can be found in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 11d: East Hodgson Park and Eco Park Concept.</i></p>
<p>Detailed Wetland Policies are contained in Sections 14.0 and 15.0 of the Waterbridge MASP and should be referred to at the Outline Plan and Land Use Amendment review stage.</p>	<p>See the Staged Master Drainage Plan that has been submitted to the City under separate cover.</p>
<p><b>11.2 ROAD NETWORK</b></p>	
<p><b>11.2.2 Policies</b></p>	
<p><b>(1) Road Network Design</b></p>	
<p>The specific road network design shall be determined through the Outline Plan process.</p>	<p>The proposed road network is shown in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 21a: Transportation Plan.</i></p>
<p><b>(2) Road Network Standards</b></p>	
<p>(a) The commercial street within the High Street Area and Chestermere Boulevard should be based on specialized standards tailored to the function of the road with the standards incorporating treed boulevards where possible.</p>	<p>The standard for Chestermere will be determined through discussions with the City.</p>
<p>(b) The balance of the roads,</p> <p>(i) should comply with the approved City standards;</p> <p>(ii) may contain special standards where determined necessary.</p>	<p>The road all comply with City of Calgary standards.</p>
<p><b>(4) Road Network Analysis</b></p>	
<p>In conjunction with an Outline Plan and Land Use Amendment application the following analysis should be provided:</p> <p>(i) an updated Transportation Impact Assessment that addresses the capacity and design of the road network needed to serve the proposed land use pattern; and</p> <p>(ii) special road cross-section for the commercial streets and Chestermere Boulevard showing the proposed design standards for these roadways.</p>	<p>An updated Transportation Impact Assessment has been provided under separate cover to the City.</p> <p>Road cross-section within the Chelsea outline plan area comply with City of Calgary standards.</p>
<p><b>11.3 TRANSIT SERVICE</b></p>	
<p><b>10.3.2 Policies</b></p>	
<p><b>(1) Transit Routes</b></p>	
<p>At the Outline Plan stage, bus transit routes and</p>	<p>Proposed bus transit routes and stops are shown in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 22: Transit Network.</i></p>

Applicable Policies	Conformance
stops will be defined.	
<p><b>(3) Transit Coverage Analysis</b></p> <p>In conjunction with an Outline Plan and Land Use Assessment application a Transit Coverage Assessment showing transit routes and stops, and the transit coverage should be provided.</p>	<p>A Transit Coverage Assessment, showing transit routes, stops, and coverage, is shown in <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 22: Transit Network</i>.</p>
<b>11.3 PATHWAY SYSTEM</b>	
<b>11.3.2 Policies</b>	
<p><b>(1) Regional Pathway Alignment</b></p> <p>The regional pathway should be aligned along the north/south open space corridor that extends through the Plan Area and with the existing pathways in the City to the east.</p>	<p>The proposed regional pathway runs north/south along the east boundary of the subject site and through the proposed open space system composed of linear parks, the High School and the ecopark.</p>
<p><b>(2) Local Pathway Alignment</b></p> <p>The alignment of local pathways should be determined at the Outline Plan stage.</p>	<p>Local pathways are identified on <i>Chelsea Outline Plan &amp; Land Use Redesignation, Figure 19: Outline Plan</i>.</p>
<b>12.2 SANITARY SEWER COLLECTION</b>	
<p>Detailed Sewer Servicing Policies are contained in Section 18.0 and 19.0 of the Waterbridge MASP and should also be referred to at the Outline Plan and Land Use Amendment review stage:</p> <p><i>19.1.2 Policies</i></p> <p><i>(1) Municipal Utilities</i></p> <p><i>(a) Urban development shall be serviced with piped municipal water and sanitary sewer utilities.</i></p> <p><i>(b) The alignment and capacity of water distribution and feeder mains and sanitary sewer trunks and collection mains should be in accordance with established Town standards, based upon the utility servicing studies and analysis undertaken and approved for the subject area.</i></p> <p><i>(c) Utility rights-of-way and easements shall be required to accommodate municipal utilities as determined necessary.</i></p> <p><i>(d) Notwithstanding other policies in this Master ASP</i></p> <p><i>i. easements, public utility lots and road rights-of way shall be required to be pre-dedicated and registered across undeveloped land to facilitate orderly and sequential development within different ownership areas, and;</i></p> <p><i>ii. may be pre-dedicated and registered or cross undeveloped land in advance of the approval of an ASP or Outline Plan and Land Use Amendment on the subject lands.</i></p> <p><i>(e) Subject to standard cost recoveries, a developer may be required to finance, or enter into an</i></p>	<p><i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 10.2: Sanitary Servicing</i> presents initial details on the proposed sanitary system to service the outline plan area.</p>

Applicable Policies	Conformance
<p><i>agreement to finance when required, the costs associated with the extension of municipal utilities through or adjacent to a site in advance of development in order to allow for the servicing of the developer's land.</i></p> <p><b>19.3.2 Policies</b>  <b>(2) Staged Sewage Collection System</b></p> <p><i>As the existing sewage collection system will be supplemented with the ultimate system for Waterbridge, in conjunction with the approval of each Area Structure Plan, and, if necessary, a land use amendment application, a staged sewage plan should be submitted that determines:</i></p> <p><i>(a) the maximum servicing capacity of the existing sewage collection system,</i></p> <p><i>(b) the staged improvements necessary to serve the subject lands,</i></p> <p><i>(c) the method of land acquisition for any off-site sewage collection system, and the responsibility and costs of construction for the land and improvements, and</i></p> <p><i>(d) any other information determined necessary to ensure the provision of adequate services.</i></p>	
<p><b>12.3 STORMWATER MANAGEMENT</b></p>	
<p><b>12.3.2 POLICIES</b></p>	
<p><b>(2) Staged Master Drainage Plan</b></p>	
<p>A staged Master Drainage Plan shall be submitted in conjunction with an Outline Plan and Land Use Amendment application to demonstrate a specific stormwater management design solution can be implemented in accordance with the approved Master Drainage Plan for the Plan Area and the City's Interim Stormwater Management Plan.</p>	<p>A Staged Master Drainage Plan has been submitted to the City under separate cover.</p>
<p>Detailed Stormwater Management Policies are contained in Section 14.0 and 15.0 of the Waterbridge MASP and these policies should be referred to at the Outline Plan and Land Use Amendment Review stage:</p> <p><b>15.1.2 Policies</b>  <b>(1) Primary Stormwater Management System</b>  <b>(a) Subject to (2) and (3) below:</b></p> <p><i>i. stormwater from Waterbridge may be discharged to the south into the Shepard Ditch once it becomes operational in accordance with the Shepard Regional Drainage Plan, or other plans that amend,</i></p>	<p>See the Staged Master Drainage Plan that has been submitted to the City under separate cover.</p>

Applicable Policies	Conformance
<p><i>repeal or replace that plan in the future;</i></p> <p><i>ii. the Stormwater Drainage System within Waterbridge shall be designed to facilitate compliance with the Shepard Regional Drainage Plan.</i></p> <p><i>(2) Alternative Stormwater Management Systems</i></p> <p><i>(a) Notwithstanding (1), as an alternative or complementary stormwater option to the Shepard Regional Drainage Plan proposal, the Town shall investigate and, if determined feasible, implement one of the following stormwater management systems in advance of the operation of the Shepard Drainage System, in order to allow development to proceed within Waterbridge:</i></p> <p><i>i. in season release to the Western Irrigation District (WID) Canal as per WID Stormwater guidelines;</i></p> <p><i>ii. drainage scenarios as recommended through the CSMI process;</i></p> <p><i>iii. end of season release to the WID Canal as per WID Stormwater guidelines;</i></p> <p><i>iv. an alternate management scenario that provides a regional short or long term solution for Waterbridge.</i></p> <p><i>(3) Interim Stormwater Management Systems Until such time as a permanent Stormwater Management System is implemented, interim solutions may be introduced on a staged basis as development proceeds in Waterbridge that include, but are not limited to:</i></p> <p><i>i. irrigation or evaporation measures that serve selected development stages to operate under zero discharge conditions;</i></p> <p><i>ii. design of stormwater facilities to contain the continuous accumulation of stormwater onsite during the irrigation season, allowing for discharge to the WID canal at the end of season, subject to approval by the WID.</i></p> <p><i>(4) Stormwater Release</i></p> <p><i>The Stormwater Discharge System should comply with the following parameters:</i></p> <p><i>i. stormwater flows generated from development in Waterbridge will not be allowed to discharge into the West Creek Basin, Chestermere Lake or the Western Headworks (WH) Canal, unless the WID amends their current policies;</i></p> <p><i>ii. stormwater flows to the WID canal following treatment to WID stormwater quality standards;</i></p> <p><i>iii. interim stormwater management solutions will need to comply with established minimum discharge</i></p>	

<b>Applicable Policies</b>	<b>Conformance</b>
<p><i>rates that, in practical terms, equate to a “zero release” level.</i></p> <p><i>iv. Emergency overland escape routes shall be determined by the Stormwater Management report at the Outline Plan stage.</i></p> <p><b>(5) Best Management Practices</b></p> <p><i>Best Management Practices (BMP’s) and Low Impact Development (LID) measures should be implemented in order to improve the quality of stormwater and, of equal importance, comply with the discharge rate requirements.</i></p> <p><i>The feasibility of stormwater re-use for irrigation may be explored at the Outline Plan stage.</i></p>	
<b>13.1 PHASING</b>	
<b>13.1.2 Policies</b>	
<b>(1) Phasing Plan</b>	
<p>(a) In conjunction with an Outline Plan application, a phasing plan may be required to be provided.</p>	<p><i>Chelsea Outline Plan &amp; Land Use Redesignation, Section 12.1: Phasing</i> presents a phasing plan for the subject site.</p>
<b>13.2 OUTLINE PLAN / LAND USE AMENDMENT APPLICATION</b>	
<b>13.2.2 Policies</b>	
<p><b>(1) Land Use Amendment Approval</b></p> <p>An Outline Plan and Land Use Amendment may be submitted and approved for all or any portion of lands within this ASP area.</p>	<p>The subject land use and outline plan submission represents the Chelsea area.</p>



## Appendix B: Chelsea Commercial Design Guidelines

## **Chelsea Commercial Design Guidelines**

The Chelsea Commercial Design Guidelines will ensure that the Chelsea Commercial Centre (Chelsea Centre) meets the high quality design standards required by the City of Chestermere, is sensitive to adjacent uses, and provides a vibrant and attractive commercial area.

Chelsea Centre is envisioned to be a community focal point that will provide daily commercial and service needs in a walkable, human scaled commercial centre. Grocery, retail, service and restaurant sites are primarily located on the south and west sections of the commercial site, enabling walkability between these services and the residential areas. Larger parking areas are located north of the main service and shopping area. To ensure an attractive interface with Chestermere Boulevard, additionally some commercial buildings will be located directly adjacent to Chestermere Boulevard to buffer the visual impact of the parking areas. All parking areas will conform to parking lot design requirements found in the Chestermere Design Guidelines.

Some specific guidelines are outlined below, however all Chestermere Design Guidelines for commercial areas will apply to Chelsea Centre.

### ***General Guidelines:***

- Setbacks, site coverage and lot restrictions will be governed by the land use bylaw.
- Landscaping will conform to the Chestermere Landscape Design Guideline Manual and the Chestermere Design Guidelines.
- Form and character of commercial buildings will align with character suitable to the Town of Chestermere and be of compatible architectural style to the community of Chelsea.
- Commercial buildings shall be design by a registered architect and landscape design by a registered landscape architect.
- All building materials, colours and finishes shall be coordinated and subject to approval.

### ***Requirements of the Chelsea Commercial Centre Development Permit:***

- Illustration of how the development has been designed to mitigate the impact of traffic, noise, lighting, and other environmental considerations on surrounding residential development.
- Illustration that the exposure of all four sides of a building was considered in the design and site planning.

- Steps taken to ensure that building facades that back onto residential areas or are located along major roadways have been designed to address visual impacts including an attractive façade and landscaping.
- Landscaped areas along all sides of the property that abut streets and adjoining adjacent development so that there is an integration of landscape treatment and provide appropriate/sensitive buffering. No blank walls will be permitted.
- Sidewalk shall connect buildings, create common open space for meeting or gathering, and create attractive sidewalks and landscaping.
- Public space pedestrian amenities such as benches, sitting areas, bicycle racks, pedestrian scale lighting, water features, and public art, identified.
- Identify opportunities for building site design to allow for activity to be extended onto the sidewalk and other outdoor spaces without obstructing pedestrian circulation. Examples include restaurant patios, display areas for retail stores, or sales/display areas for produce and flowers.
- Any part of a building site left vacant for future development shall be landscaped in a manner consistent with the landscape plan for the remainder of the building site or surrounding sites.
- The individual entries on multiple tenant buildings shall each be identifiable without detracting from the buildings overall appearance.
- Buildings and attractive landscaping features shall be placed in high visibility corner locations.
- Parking, whether for visitors or employees, should be offset from Chestermere Boulevard and mainly provided to the side, rear or internally within the property. In areas where buildings do not front Chestermere Boulevard and openings exist, appropriate landscaping elements should be incorporated to reduce the exposure to the parking lot, while still providing necessary sightlines to retail buildings and signage.
- Parking shall be shared between uses to minimize size and unnecessary replication.
- Delivery entrances for heavy vehicles, service vehicles, and trucks should be located away from residential areas. Where this site location is not possible, provide additional landscape and fence buffering to minimize noise, fumes, and other potential impacts.

- Create inviting entrance and plaza or courtyard opportunities especially on corner sites for commercial uses.
- Ground floors along the street fronts shall have a minimum transparency (windows and associated glazing) of 30-40%.
- Main building entries should provide weather protection that is designed to be an integral feature of the building's architectural character.
- In addition to the above, landscaping, signage and lighting shall adhere to the Chestermere land use bylaw and Design Guidelines.